



VILLAGE OF MOUNT HOREB

E. Main Street

Mount Horeb, WI 53572

Phone: (608) 437-6884 Fax: (608) 437-3190

Email: mhinfo@mounthorebwi.info Web: mounthorebwi.info

VILLAGE BOARD AGENDA

Wednesday, January 7, 2026 at 7:00 PM

Senior Center
107 N. Grove St.
Mount Horeb, WI

- 1) Call to order
- 2) Pledge of Allegiance
- 3) Roll call
- 4) Public Comments on Non-Agenda Items*
- 5) Consent Agenda
 - a. Consideration of December 03, 2025 Meeting Minutes
 - b. Consideration of Resolution 2026-01 Recognition of World Migratory Bird Day
 - c. Consider Approval of Appointed Election Officials for the 2026-2027 Election Cycle
 - d. Consideration of Alcohol Beverage License application for Rokit, LLC, DBA Sunn Cafe, 201 E Main Street
 - e. Consideration of Alcohol Beverage Appointment of Agent for Taddeus Rokitowski, Rokit LLC, DBA Sunn Cafe, 201 E Main Street
- 6) Agenda Items
 - a. Swearing in of new Police Officer, Samuel Erickson
 - b. Langhaus on Main Mixed-Use Building, 208-220 E Main Street
 1. Consideration of General Development Plan, Planned Development Rezone Application, Jeff Grundahl, Langhus on Main, 208-220 E Main Street
 2. Consideration of Certified Survey Map to Combine Lots of 208, 214 and 220 E Main Street
 3. Consideration of Specific Implementation Plan, Jeff Grundahl, Langhus on Main, 208-220 E Main Street
- 7) Committee Reports:
 - a. Community Development Authority

- b. Dane County Cities & Villages Association Report
 - c. Finance/Personnel
 - d. Historic Preservation Committee Report
 - e. Library Board
 - f. Mount Horeb Area Chamber of Commerce
 - g. Mount Horeb Area Joint Fire Department
 - h. Parks, Recreation, and Forestry Commission
 - i. Plan Commission
 - j. Public Safety Committee
 - k. Public Works Committee
 - l. School Liaison
 - m. Sustainability and Natural Resources Committee
 - n. Tourism Commission
 - o. Utility Commission
- 8) Village President's report
 - 9) Village Administrator's report
 - 10) Village Clerk's report
 - 11) Closed Session
 - a. Consideration of TIF Incentive Request (Jeff Grundahl Main Street Mixed Use Project). The Village Board may convene in closed session as authorized by Wisconsin Statute 19.85(1)(e) for the purpose of deliberating or negotiating the investing of public funds or conducting other specified public business, whenever competitive or bargaining reasons require a closed session.
 - 12) Meeting adjournment.

***Public Comment Policy**

Members of the public are invited to speak at meetings of all Mount Horeb Public Bodies. To comment, please complete a Public Comment Form at the Meeting Room entrance and submit it to staff before the meeting begins. Comments are limited to **three minutes**, must be made from the podium, and the speaker must return to the audience after speaking.

- **Non-agenda item comments** are heard at the start of the meeting. Public Body members and staff will not engage in discussion during public comment but may consider topics for future agendas.
- **Agenda item comments** are heard during the relevant item, after the proposers or staff present the item and before Public Body discussion. All public comments on the item will be heard before any discussion by the Public Body.

Members of the public will only be allowed to speak outside these public comment times if they are invited by the meeting chair to share additional information requested by the Public Body. If so invited to speak, the member of the public must do so from the podium.

Written comments are also welcome. Written comments shall include the name and address of the submitter and should be submitted to the Deputy Treasurer/Governance Coordinator by email at niki.erickson@mounthorebwi.info (subject line: *Public Comment Request-Name of Public Body*) or delivered by to the Village at: 138 E Main Street, Mount Horeb WI, 53572, ATTN *Public Comment Request-Name of Public Body*.

UPON REASONABLE NOTICE, EFFORTS WILL BE MADE TO ACCOMMODATE THE NEEDS OF DISABLED INDIVIDUALS THROUGH APPROPRIATE AIDS AND SERVICES. FOR INFORMATION OR TO REQUEST THIS SERVICE, CONTACT ALYSSA GAFFNEY, CLERK, AT 138 E MAIN STREET, MOUNT HOREB, WI (608) 437-9404.

**VILLAGE OF MOUNT HOREB
VILLAGE BOARD MEETING MINUTES
DECEMBER 3, 2025**

The Village Board met in regular session in-person on the above date in the Board Room of the Mount Horeb Municipal Building.

Call to Order/Roll Call: Village President Ryan Czyzewski called the meeting to order at 7:00pm. The Pledge of Allegiance was recited. Present were Trustees Jones, White, Fendrick, Schellpfeffer, Gauger, and Best. Also present were Administrator Nic Owen, Village Clerk Alyssa Gaffney, and Youth-In-Government student Jenna Jones.

Owen explained the changes to the agenda, from its original publication.

Public Comments: Village resident Bill McInnes voiced his concerns about school bullying.

Consent Agenda: Fendrick requested the November 19th meeting minutes, Final Pay Request for Nesheim Water Main project, and Appointment of Aaron Fendrick to Youth in Government program be removed from the consent agenda. Jones moved, White seconded to approve the following remaining consent agenda items: November 5, 2025 meeting minutes; November 5, 2025 Joint Finance & Personnel and Village Board meeting minutes; Pay Request 25-101-6 FINAL, Street Project 2025, Rule Construction; Finance & Personnel Committee decision regarding Dog & Cat Licensing fee increase; Facade and Building Improvement Grant for Olson's Flowers/Melrose on Main, 400 E Main Street; Alcohol License application for Melrose on Main, LLC, 400 E Main Street; Alcohol License Agent application for Kristen Dresen, Melrose on Main, LLC, 400 E Main Street, and Resolution 2025-17 Authorization of Pedestrian Facilities for LRIP Grant application. Motion approved by unanimous voice vote. Fendrick pulled the November 19th minutes to recuse himself from the vote, since he was not present at that meeting. Schellpfeffer moved, Gauger seconded to approve the November 19th meeting minutes. All voted aye, except for Fendrick, who had recused himself. Motion carried. Fendrick pulled the Final Pay Request for the Nesheim Water Main project due to the complaints received at the Utility Commission meeting. Owen addressed Fendrick's concerns about the complaints. Fendrick moved, Jones seconded to approve the pay request. Motion carried by unanimous voice vote. Fendrick pulled the Appointment of Aaron Fendrick to recuse himself from the vote, due to a conflict of interest. Gauger moved, White seconded to approve the Appointment of Aaron Fendrick to the Youth-In-Government program. All voted aye, except for Fendrick, who had recused himself. Motion carried.

Consideration of Award for Assessor Contract-Tyler Technologies: Owen explained this item. The board members discussed. Fendrick moved, Schellpfeffer seconded to approve the award. Motion carried by unanimous voice vote.

Consider Conditional Use Permit Application, Certified Survey Map, Site Plan and Design Review for the expansion of the Mount Horeb Elementary School in the R-1 Single Family Zoning District from the Mount Horeb Area School District, 207 Academy St: Owen explained this item. Gauger moved, White seconded to approve the Conditional Use Permit application. Motion carried by unanimous voice vote. Jones moved, Schellpfeffer seconded to approve the Certified Survey Map to combine the lots. Motion carried by unanimous voice vote. Best moved, Fendrick seconded to approve the Site Plan and Design Review for the addition, with the inclusion of the height adjustment. Motion carried by unanimous voice vote.

Committee reports: All committee reports were given, with no action taken.

Village President's report: Czyzewski had provided a written report and stated the contract for SW Dane Outreach was received. He also wished everyone happy holidays.

Village Administrator's report: Owen spoke about the meeting location next month, tax bills going out in the mail next week, and the Lukken proposals.

Village Clerk's report: Gaffney stated that candidacy papers are now available and to contact her if interested.

Adjournment: With no further business before the board, Jones moved, Best seconded to adjourn @ 7:55pm. Motion carried by unanimous voice vote.

Minutes by Alyssa Gaffney, Village Clerk



AGENDA ITEM REPORT

MEETING DATE

January 7, 2026

PREPARED BY

Nicholas Owen, Administrator

AGENDA ITEM # 5.b

Consideration of Resolution 2026-01 Recognition of World Migratory Bird Day

BACKGROUND

The attached resolution is recommended by the Sustainability and Natural Resources (SNR) Committee for approval, drafted by committee member Kerry Beheler, adopts May 9th as World Migratory Bird Day as a requirement of a Bird City Application. The annual Bird City fee has been paid out of the SNR Budget and Beheler is putting the final touches on the formal application prior to submitting it to the Bird City website. From a preliminary review by the Bird City program liaison, we already have enough points for acceptance into the program but will potentially have enough points for a higher recondition category when the application is complete. By joining the Bird City program, Mount Horeb can gain more interest from birders and help draw additional tourists.

RECOMMENDATION

ATTACHMENTS

1. reso 2026-01 World Bird Day

RESOLUTION 2026-01

Recognizing World Migratory Bird Day Resolution

Whereas, migratory birds are some of the most beautiful and easily observed wildlife that share our communities, *and*

Whereas, many citizens recognize and welcome migratory songbirds as symbolic harbingers of spring, *and*

Whereas, these migrant species also play an important economic role in our community, controlling insect pests and generating millions in recreational dollars statewide, *and*

Whereas, migratory birds and their habitats are declining throughout the Americas, facing a growing number of threats on their migration routes and in both their summer and winter homes, *and*

Whereas, public awareness and concern are crucial components of migratory bird conservation, *and*

Whereas, citizens enthusiastic about birds, informed about the threats they face, and empowered to help address those threats can directly contribute to maintaining health bird populations, *and*

Whereas, since 1993 World Migratory Bird Day (formerly International Migratory Bird Day) has become a primary vehicle for focusing public attention on the nearly 350 species that travel between nesting habitats in our communities and throughout North America and their wintering grounds in South and Central America, Mexico, the Caribbean, and the southern U.S., *and*

Whereas, hundreds of thousands of people will observe WMBD, gathering in town squares, community centers, schools, parks, nature centers, and wildlife refuges to learn about birds, take action to conserve them, and simply to have fun, *and*

Whereas, while WMBD officially is held each year on the second Saturday in May, its observance is not limited to a single day, and planners are encouraged to schedule activities on the dates best suited to the presence of both migrants and celebrants, *and*

Whereas, WMBD is not only a day to foster appreciation for wild birds and to celebrate and support migratory bird conservation, but also a call to action,

NOW THEREFORE I, Ryan Czyzewski, as President of the Village of Mount Horeb, do hereby 9 May 2026 as **World Migratory Bird Day** in the Village of Mount Horeb, and I urge all citizens to celebrate this observance and to support efforts to protect and conserve migratory birds and their habitats in our community and the world at large.

The above and foregoing Resolution was duly adopted at a meeting of the Board of Village of Mount Horeb on January 7, 2026.

Ryan Czyzewski, Village President

ATTEST:

Alyssa Gaffney, Village Clerk



AGENDA ITEM REPORT

MEETING DATE

January 7, 2026

PREPARED BY

AGENDA ITEM # 5.c

Consider Approval of Appointed Election Officials for the 2026-2027 Election Cycle

BACKGROUND

RECOMMENDATION

ATTACHMENTS

1. Appointed Election Officials- 2026-2027

APPOINTED ELECTION OFFICIALS
2026-2027 ELECTION CYCLE

<u>First Name</u>	<u>Last Name</u>
Delores	Ace
Bernette	Allemann
Leonard	Allemann
Anthony	Anthony
Jessica	Arrigoni
Kathy	Barnekow
Susan	Bartlett
Laurie	Bartolini
Barbara	Bartz
Kerry	Beheler
Ilana	Bloom
Annette	Boehnen
Joseph	Boehnen
Laurie	Boyden
Susan	Brice
William	Brink
Lori	Brody
Michelle	Campbell
Sarah	Castello
Georgann	Condrad
Carlos	Cruz
Stephen	Cummings
Ellie	Dahlquist
Cynthia	Doran
Brian	Driscoll
Polly	Field
Carleen	Fisher
Sandra	Frank
Susan	French
David	Gobeli
Jennifer	Gottwald
Amy	Gribb
Debrah	Gunderson

Mary Jo	Gundlach
Toni	Gunnison
Monica	Hale
Sandra	Hall
Marlene	Harley
William	Harley
Wayne	Haskins
Melissa	Haub
Deb	Haufschild
Michelle	Hess
Laura	Hiler
Dave	Hoffman
Sandy	Hufton
Steve	Hufton
Charlie	Jefko
Cynthia	Jefko
Beverly	Jones
Michelle	Jones
Salem	Joseph
Susan	Kaletka
Margit	Kelley
Frank	Koelbl
Phil	Leavenworth
Claire	LeBack
Renata	Lee
Jeff	Lombardo
Doug	McFarlane
Brad	McGuire
Christine	McInnes
Adam	Mertz
Amy	Mertz
Alexander	Mesenberg
Lynn	Messinger
Sarah	Midgley
Lori	Midthun
Maggie	Milcarek
Beth	Miller

Chad	Minter
Lori	Minter
Connie	Nankee
Mary	O'Donnell
Barb	Ott
John	Ott
Tom	Pedretti
Troy	Pflum
Terry	Pohle
Kay	Rhode
Michelle	Ripp
Bonnie	Rogney
Carroll	Schaal
Pam	Schaal
Renee	Schumacher
Cathy	Scott
Ken	Scott
Kate	Skogen
Charlie	Steinhauer
Noah	Strube
Sharon	Stuckey
Julie	Tallard Johnson
Nathaniel	Taft
Christine	Tanzer
James	VandenBrook
Judith	VandenBrook
Teri	Vierima
Kaleigh	Waldmann
Pete	Walton
John	Wardour
Leonard	Werner
Carolyn	White
Karen	White
Tim	White
Linda	Winther
Kathleen	Zwettler



AGENDA ITEM REPORT

MEETING DATE

January 7, 2026

PREPARED BY

Alyssa Gaffney, Village Clerk

AGENDA ITEM # 5.d

Consideration of Alcohol Beverage License application for Rokit, LLC, DBA Sunn Cafe, 201 E Main Street

BACKGROUND

Rokit LLC has purchased Sunn Cafe, and is therefore applying for an alcohol license. The previous owner held the same type of license. (Beer & Wine)

RECOMMENDATION

ATTACHMENTS

1. Alcohol Beverage Lic App-Rokit LLC



AGENDA ITEM REPORT

MEETING DATE

January 7, 2026

PREPARED BY

AGENDA ITEM # 5.e

Consideration of Alcohol Beverage Appointment of Agent for Taddeus Rokitowski, Rokit LLC, DBA Sunn Cafe, 201 E Main Street

BACKGROUND

RECOMMENDATION

ATTACHMENTS

1. Agent App- Taddeus Rokitowski



AGENDA ITEM REPORT

MEETING DATE

January 7, 2026

PREPARED BY**AGENDA ITEM # 6.a**

Swearing in of new Police Officer, Samuel Erickson

BACKGROUND**RECOMMENDATION****ATTACHMENTS**

None



MEETING DATE	PREPARED BY
January 7, 2026	Nicholas Owen, Administrator

AGENDA ITEM # 6.b

Langhaus on Main Mixed-Use Building, 208-220 E Main Street

BACKGROUND

Developer Jeff Grundahl has resubmitted his General Development Plan for a mixed-use building at 208-220 E Main Street. Revisions made since the last submittal include removing the 4th floor of the project, reducing residential units from 64 to 44, reducing parking stalls to 55 while increasing the parking ratio to 1.25 spaces per unit and one space per bedroom, and increasing the first floor commercial to over 9,000 square feet. To address safety concerns from the last proposal, Grundahl is making the Third Street garage door be an entrance only to avoid cars making a blind exit from the garage to the sidewalk on Third Street and is requesting to make the alley north of the building one-way from Third Street to Second Street. Following the recommended approval at the Plan Commission meeting, Grundahl provided some additional revisions to his plan to address safety concerns raised about his project at the meeting. Along with the plans for the project, Planner Ben Rohr has provided his project review.

Additionally, this month, Grundahl has submitted a Certified Survey Map (CSM) to combine the three existing lots into one lot and the Specific Implementation Plan (SIP), which is the design review and site plan portion of the Planned Development process. Rohr has provided a review of the SIP as well.

After another round of lengthy public comment, many opposed, but feeling the improvements made have made the project better, and few in favor, the Plan Commission voted 4-3 to recommend approval of the GDP and rezone to PD to the Board on a 4-3 vote. They also approved the CSM and SIP to the Board by 6-1 votes.

The Village takes the safety concerns of its citizens seriously and evaluates them based on the existing data we have from our Police Department, available traffic studies, Wisconsin DOT traffic data and any other available resource. Many of the concerns with this project are existing concerns with traffic on Main Street and visibility issues with the alley that runs between 2nd and 3rd Streets that the Village is aware of and working on addressing. Both the Chief of Police and Fire Chief have been



AGENDA ITEM REPORT

involved with project meetings on this proposal and have been provided with updated plans as the proposal has evolved and have not raised concerns that would cause us to stop the approval process.

Looking at existing crash data for the downtown area for cars versus pedestrians, we have had 3 in the last 5 years with an estimated 18,200,000 cars passing on Main Street during that time. Of those accidents, two of the pedestrians that were hit crossed mid-block instead of at the crosswalk and the third was hit by a car backing out of a parking stall. The most consistent issue we have seen causing accidents on Main Street during this time is with distracted driving, and the police are going to increase the focus on distracted driving to try to discourage that behavior.

The Village is also looking into alternate east-west routes to reduce Main Street traffic with future developments in the Village's Comprehensive Plan. One of the features of the Comprehensive Plan is to plan for future transportation and a new way to travel east-west in the Village is part of those plans. This is not a 5-10-year fix and is more likely a long-term 20-year fix. Our Sustainability and Natural Resources Committee is working on a Bike Friendly Community application. Part of the application includes identifying safe bike routes in the Village. The application review process also identifies areas where the Village can improve on its bike safety, which can be another piece of the transportation puzzle. The Village's Public Safety and Public Works Committees have often discussed these safety-related topics in the past and are currently working on a few safety-related topics and are willing to review and consider any other street/pedestrian/bike safety-related items.

When looking at this project and its projected traffic flow data, it will not significantly add to these concerns. While adding 44 housing units to this spot may seem significant, we need to keep in mind it is replacing a previous apartment building and two commercial buildings. When looking at the trip data generated by a mixed-use development, not all the residents will come and go from the building at the same time. Using the data provided for the existing Hoff Apartments, a 48-unit building in Downtown Mount Horeb, the peak time for vehicles leaving the property was seven vehicles between 6:45-7 each morning. The major back-up time for traffic on Main Street is during school drop off time between 7:45 and 8:00 a.m. If this building has similar demographics, we should experience similar peak traffic times. During the evening hours, return trips were even more dissipated, with three return trips from 4:15-4:30, 5:15-5:30 and 5:30-5:45. Village Engineer Rob Wright has provided comments on the original traffic analysis for the project, now that it has been reduced to 44 units.



AGENDA ITEM REPORT

I think a good approach to the traffic and pedestrian concerns, if the building is approved, is to continue to monitor the traffic data once the building is occupied and, if issues arise, make the needed changes. The Public Works and Public Safety Committees have been informed on the issues and have a good background on the situation. Those committees will continue with their existing work on monitoring downtown traffic and crosswalk safety as well and recommend any changes to the Village Board.

RECOMMENDATION

ATTACHMENTS

1. 214 E. Main Street Mixed Use Building GDP Memo 11.19.25
2. 214 E. Main Street SIP and Design Review Memo for Plan Commission 11.19.25
3. 251113_Mount Horeb Main St_LOI
4. 251126_Mount Horeb Main St_changes letter
5. 251126_Langhus on Main_Updated sheets
6. 251116_Arch Mount Horeb Submittal
7. Langus combined site plans
8. Langus_site lighting specs
9. Gallina traffic study graphs Final
10. Updated Economic Impact Analysis for 214-220 E Main



VANDEWALLE & ASSOCIATES INC.

To: Village of Mount Horeb Plan Commission
 From: Village Planner Ben Rohr, AICP
 Date: November 19, 2025
 Re: Proposed Certified Survey Map to Combine Parcels 0606-123-2382-3, 0606-123-2369-0, 0606-123-2355-6 and Zoning Map Amendment from Central Business (PB) zoning to Planned Development - General Development Plan (GDP) for a 3-Story Mixed-Use Building at 210, 214, and 220 E. Main Street for Jeff Grundahl.

Summary of Proposed Changes

The proposed project was reviewed and tabled by the Plan Commission following a public hearing and discussion on October 22, 2025. There was a significant amount of public comment during the meeting. As such, I have not included all of that testimony in this staff report, but it has all been documented within the public record associated with the October 22, 2025 meeting.

Many of the Plan Commission members shared the public’s concerns and instructed the applicant to revise the plan set to address the concerns. The applicant has provided a revised plan set that includes the following changes. This staff report has been revised to reflect these changes.

Proposed Today	Previous Submittal
3-story mixed-use building	4-story mixed-use building
Maximum height of 41’	Maximum height of 50’
44 total dwelling units	63 total dwelling units
8 studio units, 26 one-bedroom units, and 10 two-bedroom units	12 studio units, 38 one-bedroom units, and 13 two-bedroom units
55 in-building parking stalls for residential tenants	68 in-building parking stalls for residential tenants
44 in-building bicycle parking stalls	10 in-building bicycle parking stalls
2.36 floor area ratio	3.04 floor area ratio
12 stacked parking stalls for two-bedroom residential tenants	20 stacked parking stalls for two-bedroom residential tenants
Interconnected in-building parking area with entry access only from 3 rd Street and exit access only from the rear alley	Separated in-building parking area with individual access to 3 rd Street and the rear alley
9,164 square feet of ground floor commercial tenant space the full length of the building façade along Main Street	6,509 square feet of ground floor commercial tenant space along a portion of the building façade along Main Street, in-building parking along western portion of the building façade along Main Street
No shared residential amenity space (community room, exercise room, balcony)	2,900 square feet of residential amenity space (community room, exercise room, balcony)

Project Overview

Jeff Grundahl is proposing to combine three existing parcels into one new parcel to accommodate a three-story mixed-use building. The structure is proposed to include 44 multi-family dwelling units, 9,164 square feet of commercial space, and 55 parking spaces. Two of the subject properties are currently owned by the applicant and proposed developer, Jeff Grundahl (214 E. Main, LLC). The other property is currently under contract for purchase by Jeff Grundahl, but owned by Five Families, LLC.

The project is proposed to be located directly east of the Mount Horeb Telephone Company building on the block bounded by East Main Street, North 3rd Street, North 2nd Street, and the public alley. This block and its counterpart on the southern side of East Main Street serve as a transitional area of downtown development patterns. This includes blocks to the west featuring zero setback buildings and contiguous block faces that transition to the east to feature mixed setback former residential structures that have been converted into commercial businesses or mixed-use buildings. North of the subject properties and public alley also features a transition of development patterns into mixed residential neighborhoods and off-street parking lots.

Today, there are two existing structures and a vacant lot that occupy the subject properties. If approved, the two structures would be removed from the site to accommodate the proposed project. A Certified Survey Map has been submitted to combine the three existing parcels into one new parcel to facilitate the development. The new parcel would be 31,653 square feet or 0.73 acres. This process is proposed to occur simultaneously with the General Development Plan to ensure the Village's standards are met.

The proposed mixed-use building is a combination of market rate multi-family apartments (8 studio units, 26 one-bedroom units, and 10 two-bedroom units) above leased commercial space (9,164 sf). In-building vehicle parking (55 stalls) and bicycle parking (44 stalls) are provided for residential tenants on the ground floor. On-street parking is proposed to be utilized for residential guests and commercial patrons. Pedestrians would have access to the site from a planned sidewalk extension/connection along both East Main Street and North 3rd Street.

Vehicle access to the in-building parking area (55 stalls) would be provided via ingress only from 3rd Street and egress only to the public alley within an interconnected parking area. Within the parking area, 12 of the stalls will be stacked (i.e. double loaded), meaning that 6 stalls will be directly accessible from the drive aisle and the other 6 can only be accessed through the front row parking stalls. Per the applicant's submitted materials, the double loaded parking stalls are proposed to be leased to residents of the two-bedroom units.

A further breakdown of the proposed project by floor is provided below:

- 1st Floor = no dwelling units, 9,164 square feet of commercial space, 55 in-building vehicle parking stalls, and 44 in-building bicycle parking stalls
- 2nd Floor = 22 dwelling units
- 3rd Floor = 22 dwelling units

The proposed project is a redevelopment because there are existing structures on two of the lots. The subject properties are surrounded by existing development, and each are served by existing Village utilities and services. The subject properties generally slope downward from north to south, so the frontage on East Main Street is at a lower elevation than that of the northern portion of the site. This creates a situation where the building is proposed to be 41 feet tall at its peak along East Main Street, but only 35

feet tall at its peak along the rear alley. Additionally, a proposed building façade stepback has been employed to recess a portion of the 3rd floor along East Main Street and the portion of the 2nd and 3rd floors along the rear alleyway. This technique is used in buildings throughout Dane County to create an appearance at ground level of a smaller structure because the upper story is less visible from street view with the recessed façade.

Further, like many structures in Mount Horeb, in-building parking is proposed. This design technique is employed to screen the parking from view, eliminate the need for surface parking, control vehicle access to and from the building between two different access points (alley and North 3rd Street), and provide the ability to build above the parking area. The downtown setting is distinctly different than many other areas of the Village in that its more densely developed, has greater pedestrian connectivity, and the majority of parking occurs on-street or within public off-street parking lots. The proposed project aims to provide residential tenant parking within the building, but residential visitors and commercial patrons will utilize on-street or public off-street parking lots. This approach is utilized throughout downtown.

The proposed mixed-use building is unique and desirable for Mount Horeb. It features both leased commercial space and multi-family apartments within the same structure. While common in older structures within the downtown area and new structures throughout other communities in Dane County, only a few new mixed-use buildings have been proposed and approved within Mount Horeb. This type of development in this location also provides great benefits to the new residents where they can walk to nearby amenities (shopping, services, recreation, the Military Ridge Trail, parks, etc.), while also providing commercial spaces to support the economic vitality of downtown Mount Horeb.

The project has conceptually been brought before the Plan Commission in June and July for review and comment, a Neighborhood Meeting was held in August to provide public review and comment, and a public hearing and the General Development Plan (GDP) was considered at the October Plan Commission meeting.

The revised General Development Plan (GDP), if approved by the Village, would establish the new zoning on the subject property. The developer has also submitted the Specific Implementation Plan (SIP) for the project to be considered at the November Plan Commission meeting. The SIP staff report has been provided separately.

Analysis

If flexibility is granted in the General Development Plan (zoning) step, a Planned Development can exceed the minimums/maximums as indicated below and incorporate additional permitted land uses.

Land Use

The subject properties are currently zoned Central Business (CB). The property will be using the Central Business (CB) zoning district as the underlying district under the Planned Development zoning.

Section 17.43(2) lists the permitted and conditional principal and accessory uses of the CB zoning district. Currently, the CB district permits residential under the Commercial Apartment accessory land use category, requiring a commercial component on the first floor above the residential. Commercial land uses such as Office, Personal or Professional Services, Indoor Sales or Service, and Indoor Commercial Entertainment are permitted and conditional principal land uses in the CB zoning district.

The Commercial Apartment land use is defined in Section 17.20(8)(a) as: “Commercial apartments are dwelling units which are located above the ground floor of a building used for a commercial land use..., most typically an office or retail establishment. The primary advantage of commercial apartments is that they are able to share required parking spaces with nonresidential uses.”

The required standards associated with the Commercial Apartment land use include:

- The gross floor area devoted to commercial apartments shall be counted toward the floor area of a nonresidential development.
- A minimum of one off-street parking space shall be provided for each bedroom within a commercial apartment. Parking space provided by nonresidential land uses on the site may be counted for this requirement with the approval of the Zoning Administrator.

Accessory land uses are defined in Section 17.20(8) as: “land uses which are incidental to the principal activity conducted on the subject property.” In recognition of this definition, the proposed Planned Development would have to change Commercial Apartments from an accessory land use to a permitted by-right principal land use. All other existing permitted and conditional principal, accessory, and temporary land uses in the CB district would remain the same. This would include opportunities for office, retail, restaurants, etc. to be located within the ground floor commercial areas. As often occurs when there are multiple uses proposed for a single building or property, a project can have two principal uses (residential and commercial).

There is no current residential density requirement specified for the Commercial Apartment land use. However, it is listed as an accessory use, indicating that it is incidental to the principal activity being conducted on the subject property. In other cases of new mixed-use projects in Mount Horeb, the Village has utilized the approach of interpreting the ordinance to regulate density of mixed-use buildings through the Apartment land use (Section 17.20(1)(a)7.), which has the following standards:

- No more than 8 dwelling units in a building.
- Any building containing more than 4 dwelling units shall provide additional site design features such as underground parking, architectural elements, landscaping, and/or on-site recreational facilities.
- All apartment units shall be located a minimum of 30 feet from the boundary of the property.

As identified below, flexibilities will need to be approved within the PUD related to changing the Commercial Apartment land use from accessory to principal and related to the Apartment land use’s density standards of no more than 8 dwelling units per building. To note, the Apartment land use is an exclusively residential use with no commercial component. Within the PDD, it is not proposed to be a new permitted principal land use. The Village has expressed a strong desire for commercial space to be located on the ground floor along Main Street and thus, the Commercial Apartment standards would still require that to be the case. If the Apartment land use were to be granted zoning flexibility through the PDD and be an additional principal land use, then, in theory, the ground floor could be utilized as residential space, which is not desired by the community.

Bulk Dimensional Standards

Section 17.43(7) provides the bulk dimensions for the CB district:

Lot and Density Requirements**		
	Village Standards	Proposed
Min. Lot Area	2,000 sf	31,653 sf
Min. Lot Width	24'	253'
Min. Street Frontage	N/A	378'
Max. Number of Floors	4 floors	3 floors
Min. Building Height	20'	41'
Max. Building Height	50'	41'
Max. Impervious Surface Ratio	100%	98%
Max. Floor Area Ratio	3.00	2.36
Max. Building Size	N/A	74,701 sf
Building and Pavement Setbacks**		
	Village Standards	Proposed*
Min. and Max. Front or Street*	0'	6'
Min. and Max. Side (residential)*	0'	N/A
Min. and Max. Side (nonresidential)*	0'	1'
Min. Rear (residential)	0'	2'
Max. Rear (nonresidential)	0'	N/A
Min. Accessory Side	5' (property line and alley)	N/A
Min. Accessory Rear	5' (property line and alley)	N/A
Min. Paved Surface (all sides)*	0'	0' (east), 1' (west), 0' (south), 2' (north)
Min. Building Separation*	0'	1'

*Maximum permitted setback of 0 feet, except where permitted by the Plan Commission as an essential component of site design.

** The proposed project meets all of the bulk standards of CB zoning district listed above. Exceeding the maximum front or street setback, side setback, paved surface setback, and building separation can be approved as proposed if the Plan Commission determines that the proposed approach is an essential component of the site's design.

The Village Building Inspector and Fire Department provided comments on the proposed project, particularly the proposed one-foot separation between the new structure and the existing Mount Horeb Telephone Company (MHTC) building. Fire separation walls that are used for zero lot line situations will be required to meet all standards of the Building and Fire Codes. Additionally, the Fire Department has recommended that a fence be utilized within the one-foot separation area to avoid people or animals being able to access this area. If the GDP is approved, the applicant has provided a proposed solid metal gate between the two buildings as part of the SIP step.

Parking

Throughout downtown, off-street parking is not required because of the unique characteristics of this area where nearby on-street parking and public/private off-street parking lots accommodate the parking demand. Per Section 17.43(7)(b)5., on-site parking is not required in the CB District, unless identified as a requirement for a conditional use permit or planned development.

Parking Requirements in the CB Zoning District			
	Village Standards	Village Standards Based on Proposed Project	Proposed On-Site Parking Provided
Dwelling Units	None	-	55 in-building spaces
Commercial Standards	None	-	
Total		0 spaces	55 spaces

Section 17.20(8)(a) provides the minimum required off-street parking spaces for the Commercial Apartment land use. This section states, “A minimum of one off-street parking space shall be provided for each bedroom within a commercial apartment. Parking space provided by nonresidential land uses on the site may be counted for this requirement with the approval of the Zoning Administrator.”

Parking Requirements for Commercial Apartment Land Uses			
	Village Standards	Village Standards Based on Proposed Project	Proposed On-Site Parking Provided
Dwelling Units	1.0 space/bedroom	54 bedrooms = 54 spaces	55 in-building spaces
Total		54 spaces	55 spaces

Section 17.132 provides the minimum required off-street parking spaces for all other areas of the Village that are not zoned Central Business. Although there are no parking minimums in the CB zoning district, the Village may require parking as condition for approval associated with a planned development. While the table below does not illustrate the parking requirements for a proposed project within the CB zoning district, it provides the parking standards that would apply to the project in other areas of the community for context purposes.

Parking Requirements Outside of the CB Zoning District			
	Village Standards	Village Standards Based on Proposed Units	Proposed On-Site Parking Provided*
Efficiency Unit	1.0 spaces/unit	8 units at 1.0 spaces/unit = 8 spaces	55 in-building spaces
One-Bedroom Unit	1.5 spaces/unit	26 units at 1.5 spaces/unit = 39 spaces	
Two- Bedroom+ Units	2.0 spaces/unit	10 units at 2.0 spaces/unit = 20 spaces	
General Commercial Standards	1 spaces/300 sf	9,164 sf = 30 spaces	
Total		97 spaces	55 spaces

Over the years, minimum off-street residential parking minimums have been applied in a variety of ways in downtown Mount Horeb:

- Many of the older mixed-use buildings along Main Street do not provide on-site residential tenant parking and instead rely on both on-street and off-street public parking lots to supply the associated parking demand.
- For new construction (Hoff Mall and Front Street Station), alternative approaches have been taken. The approved Hoff Mall project includes 42 total dwelling units and 51 parking stalls (1.2 stalls per

dwelling unit). The approved Front Street Station project includes 94 in-building parking stalls for 63 total dwelling units (1.5 stalls per dwelling unit). The Hoff Mall project provides the same residential on-site parking ratio as the proposed project, while Front Street Station has a higher residential on-site parking ratio than the proposed project.

To better understand the parking situation downtown, the Village commissioned a Downtown Parking Study. That study found that there is not a parking problem downtown. According to the [Village's 2024 Downtown Parking Study](#), "there is a noted healthy downtown parking surplus (based on the current demand) that's evident when looking at the occupancy of public parking. Most public lots hovered around or below 50% filled in nearly all surveys."

The study further separated the downtown area into zones for analysis purposes in respect to potential redevelopment that could take place. The proposed project is located within Zone C within the Study. In Zone C, the Study found that there are currently 46 off-street spaces with 30% of the spaces filled during the non-event day peak period, and there are 42 on-street spaces that were 29% filled during this period. In total, that meant that there was a surplus of 25 parking spaces in off-street lots and 24 on-street spaces within Zone C.

The Study estimated that 10,000 square feet of new commercial space could potentially be added within this zone and that new commercial space could generate a parking demand at around 1 stall per 370 square feet. This would be an increased demand of 27 spaces. To note, the parking analysis also factored in 6 new parking spaces for residential tenant use within Zone C, which added an additional 6 spaces to the total increased demand (33). In total, the Study found that if a parking demand of 33 new parking spaces were generated by future redevelopment in this zone, the parking network would still have a surplus of 16 spaces during the non-event peak period. The proposed project includes 9,164 square feet of commercial and removes approximately 4,600 square feet of existing commercial space within the two structures today. The result is an increase of 4,564 square feet, which is approximately 5,436 square feet less than the Downtown Parking Study estimated for new commercial space to be added within Zone C. Based on the findings of the Downtown Parking Study, there is sufficient capacity within this area to accommodate the new commercial demand proposed during the non-event peak period.

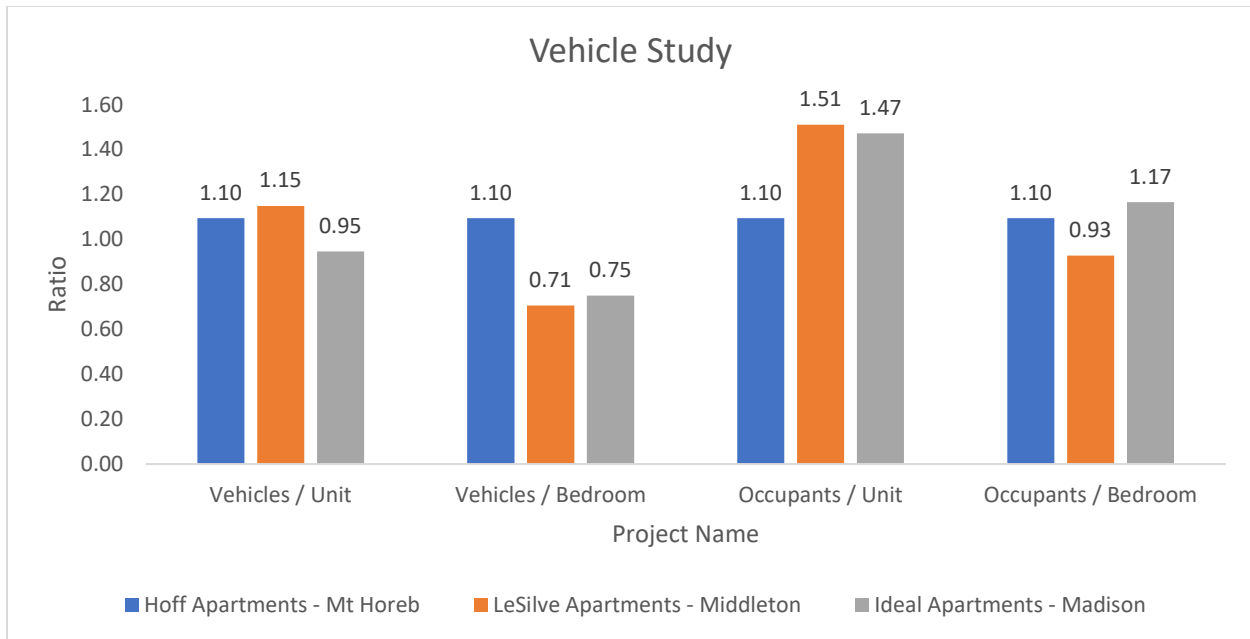
Additionally, no on-site parking is proposed for use by residential visitors. As documented within the [Downtown Parking Study](#), there is sufficient existing surplus of off-street and on-street parking spaces that could be utilized to accommodate this demand.

The proposed project provides 55 in-building parking spaces for 44 total residential units, which is a ratio of 1.25 stalls/unit. Additionally, there are a total of 54 bedrooms proposed within the 44 total residential units, which is a ratio of 1.02 stalls/bedroom. Finally, there are a total of 44 bicycle parking stalls proposed within the in-building parking area, which is a ratio of 1.0 bicycle parking stalls/unit.

Furthermore, one reason why parking demand may be lower downtown than in other areas of the community can be attributed to the demographic make-up of those choosing to live within a downtown setting. According to the [Dane County Regional Housing Strategy](#), in 2021, rental-occupied household sizes were smaller across Dane County than owner-occupied units (2.47 owner-occupied versus 1.98 rental-occupied). While admittedly anecdotal and not based on a study or quantitative analysis, the personal preferences of those choosing to live within a more urban setting (such as the proposed project) may be different than those choosing to live in a more suburban setting in other areas of the Village. For those looking to live within walking distance of many different amenities and a more densely developed

portion of the community, renters may be more likely to make the tradeoff for less parking and only have one vehicle. These residents may also skew less middle age and reflect older occupants looking to downsize or young professional occupants looking for a low maintenance living opportunity.

The developer has collected data and provided a parking analysis from existing projects managed by Gallina Properties in Dane County. It found that the actual occupants per bedroom for each of these projects was between 0.93-1.17 and the actual vehicles per bedroom was between 0.71-1.10. This data helps illustrate that there is potentially a lower demand for parking than the existing Village parking requirements (for other areas outside of the CB zoning district).



Source: Gallina Properties, 2025

An additional resource that can assist in determining the anticipated parking demand associated with a project like this includes the Institute of Transportation Engineer’s (ITE) Parking Generation Manual. [According to the ITE Parking Generation Manual, 6th Edition, 2023](#), the ITE Land Use Code 221 (Multi-Family Housing) has an average rate of parking demand expected to be generated at 1.23 vehicles per unit. This ratio is in line with the proposed project.

In total, it would be insufficient to assume that all of the proposed project’s parking demand could be met by the off-street and on-street parking supply that exists within downtown Mount Horeb today. However, based on the number of available off-street and on-street parking spaces identified within the [Downtown Parking Study](#), the demand associated with the proposed project’s commercial space and residential visitors could be accommodated.

When it comes to the residential tenant parking demand associated with the proposed project, a reduced parking ratio may be best suited as the standard for a project such as this (lower than the Village’s parking standards for other areas of the community outside of the CB zoning district). This may be appropriate for a number of reasons: 1) the downtown is a vibrant place designed for people not cars and parking lots, as evidenced by the [Comprehensive Plan](#), [Downtown Plan](#), and the Central Business zoning district’s purpose statement and parking requirement, 2) the [Downtown Parking Study](#) indicates a surplus of

available supply to support residential visitors and commercial patrons, 3) downtown renters may be more likely to be one car households, and 4) national average data suggests that the existing Village parking requirements (for other areas outside of the CB zoning district) are higher than the demand associated with multi-family uses.

As noted above, per Section 17.43(7)(b)5., on-site parking is not required in the CB District, unless identified as a requirement for a conditional use permit or planned development. As the Plan Commission considers that amount of parking needed for this project, one of the following standards could be applied:

- **Central Business Zoning District Parking Standards: None**
 - Proposed development exceeds the minimum parking requirements by 55 spaces
- **Commercial Apartment Land Use Parking Standards: 1 space per dwelling unit**
 - Proposed development meets the minimum parking requirements (54 required and 55 provided)
- **Village Parking Standards (outside of the CB Zoning District): 1 space per studio unit, 1.5 spaces per 1-bedroom unit, and 2 spaces per 2+ bedroom unit, and 1 space per 300 square feet of commercial area**
 - Proposed development would need an additional 42 spaces (97 required and 55 provided)
- **ITE Land Use Code Parking Averages: 1.23 spaces per dwelling unit**
 - Proposed development meets the minimum parking requirements (1.23 spaces per 44 dwelling units = 54 required and 55 provided)

As indicated within the proposed Village Planner's Recommendations Regarding the GDP below, I would recommend that the project be required to meet a 1.25 spaces per dwelling unit standard (no additional stalls required) and meet a 1.0 bicycle parking spaces per dwelling unit standard (no additional stalls required).

Central Business Zoning District

Section 17.43(1) includes the purpose statement of the Central Business Zoning District. It states, "This district is intended to permit both large- and small-scale downtown commercial development at an intensity which provides significant incentives for in-fill development, redevelopment, and the continued economic viability of existing development. To accomplish this effect, minimum landscape surface ratios (LSRs) permitted in this district are much lower than those allowed in the Planned Business and General Business Districts. A wide range of office, retail, and lodging land uses are permitted within this district. In order to ensure a minimum of disruption to residential development, no development within this district shall take direct access to a local residential street or a residential collector street. No requirements for on-site landscaping or parking are required in this district. This district is strictly limited to the central Village locations. This district is intended to provide an alternative, primarily in-fill development, designation for commercial activity to the Planned Business (PB) District to assist in maintaining the long-term viability of the central Village."

As noted above, the proposed project reflects many attributes of the purpose statement including being a redevelopment project, containing a combination of commercial and residential within a mixed-use building, utilizing the entire proposed lot, and assisting to maintain the long-term viability of the central Village. It also states that no direct access should be taken from a local residential street or residential collector street. After confirming with the Village Engineer, there is no official Village map that depicts street classifications. The Village Engineer views the portions of 2nd and 3rd Street directly adjacent to the lots that front onto Main Street as not falling within the classification of a local residential street or

residential collector street. He views each street as a local residential street north of the existing alley in the rear of the subject properties. As such, the proposed project is consistent with the purpose statement of the Central Business District.

Further, in Section 17.43(8), there are a special set of standards that apply within the Central Business Zoning District. This includes, “No person shall perform, cause, or permit the exterior construction, alteration or remodeling or the removal, movement, or demolition of any building, structure, or other improvement on premises located within the CB Central Business District without determination from the Plan Commission that the proposed development or alteration corresponds with the typical existing layout of the Central Business District and is compatible with the existing characteristics of the area and/or original design of the improvement. In its determination, the Plan Commission shall take into consideration the special character of the downtown and shall promote the preservation of the traditional Main Street facade and its elements of design and architectural features. Those features would include, but would not be limited to; building width, the rhythmic characteristic pattern of bays or windows, building heights, horizontal bands or elements, proportions of new buildings to old, proportion and window proportion, rhythm of solids and voids, materials and color, parking, and side, front and rear setbacks. All considerations will promote the historic and architectural qualities of the downtown and act to preserve its original distinctive character.”

This standard is best applied in the Specific Implementation Plan (SIP) stage, because it deals with the detailed design characteristics of the structure. However, there is inherent overlap between the building’s design and the General Development Plan.

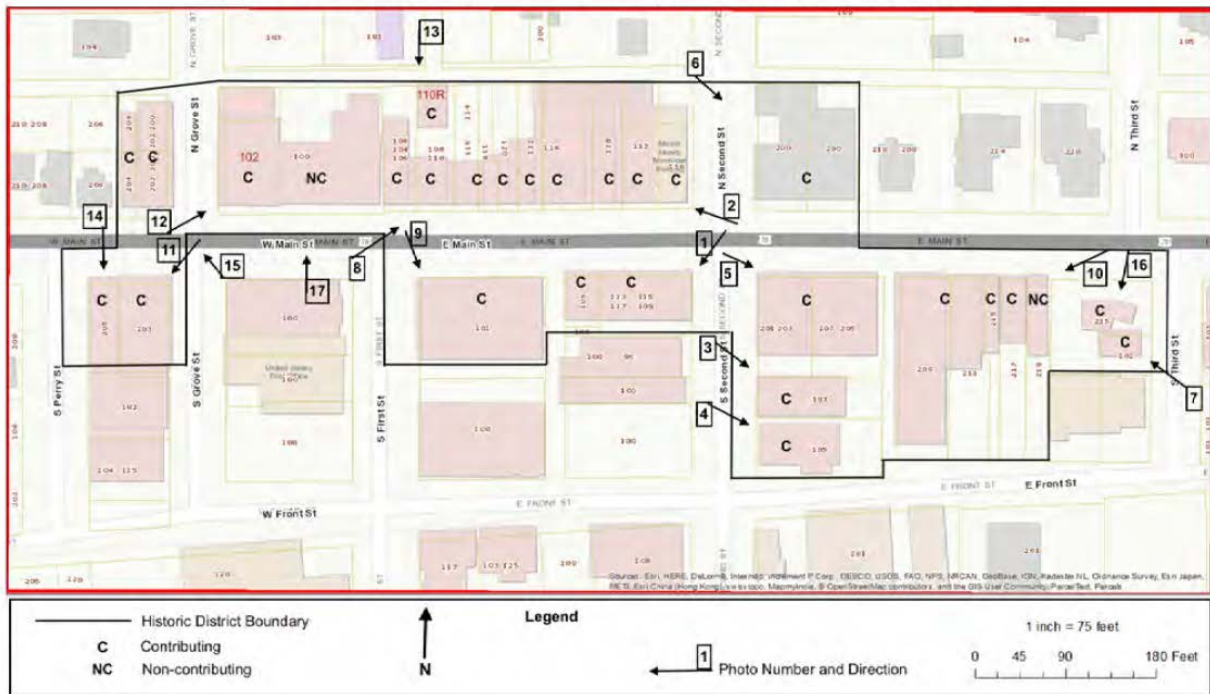
As noted above, the proposed project meets the vast majority of the Central Business District’s land use and bulk dimensional requirements. There are flexibilities required to accommodate certain aspects of the project such as the number of dwelling units per building and Commercial Apartments being a permitted principal land use. There is also a parking standard determination to be made by the Plan Commission.

While this project is unique, it also features many elements of other recently approved and/or constructed projects in Mount Horeb. This includes ground floor level in-building parking (Hoff Building, Duluth Trading Company Headquarters, Front Street Station, and 1881 Springdale Street), similar densities (Hoff Building and Front Street Station), and mixed-use commercial and residential buildings (1881 Springdale Street). It is also shorter in height than Front Street Station (60 feet), Duluth Trading Company Headquarters (85 feet to peak), the Hoff Building (47 feet), and the Opera House (35 feet to gutter, 50 feet to peak).

Historic Designation

Downtown Mount Horeb was designated by the [United States Department of the Interior as a Main Street Historic District in 2019](#). The district includes 29 total structures within six blocks of downtown Mount Horeb. Below is a map of the district from the nomination forms. The subject properties are not included within the district.

Figure 2. Main Street Historic District, Mount Horeb, Dane County, Wisconsin - District Map with Photo Key



Source: Village of Mount Horeb Main Street Historic Designation nomination, 2018.

The Village’s last historic survey was completed in 1997. The survey included both 212 and 220 East Main Street and found that neither structure is eligible for historic designation. 212 East Main Street is considered to be a contributing element in the potential locally-designated Main Street Historic District, but 220 East Main Street is not a contributing element. Below are excerpts from the survey related to the subject properties.

- 212 East Main Street. This frame Foursquare house was erected for Elef Severson in 1910 (Village of Mount Horeb Tax Rolls). It is a fine and intact example of a Foursquare and features cottage windows with leaded glass. The house remained in the Severson family until 1964, when William and Muriel Olson purchased it for their florist shop (Mount Horeb: Presettlement to 1986, p. 30). Olson’s Flowers was still in the building in 1996. The Olsons put an addition on the rear and east of the building. This property was owned by Elef Severson from 1910 until at least 1935, by John Severson from at least 1940 until at least 1945, by Emma Severson from at least 1950 until 1964, and by William and Muriel Olson since 1964. The Severson House is not individually eligible for historic designation, but is a contributing element in the potential locally-designated Main Street Historic District.
- 220 East Main Street. This frame two-story cube house was probably built by carpenter Ole Granum in 1883 to serve as his residence (Village of Mount Horeb Tax Rolls; Blue Mounds Weekly News, October 23, 1883). Railroad and telegraph agent Albert W. Gordon and his wife Elizabeth bought the house in 1886 (Blue Mounds Weekly News, May 12, 1886). Gordon was the son of Madison architect J.O. Gordon, who designed the Mount Horeb Opera House. The Gordons substantially expanded this house in 1896 (Village of Mount Horeb Tax Rolls). The Gordons lived in the house until at least 1907. A later resident was Teman Gordon, who resided here from 1911 until his death in late 1920; it remained in his family until at least 1940 (Village of Mount Horeb Tax Rolls). William and Muriel

Olson bought the property circa 1975, opening the Christmas gift shop in the building in 1977. The Olsons put a large addition on the front of the building in 1987 (building permit, July 15, 1987). This property was owned by Ole Granum from 1883 until 1886, by Albert W. Gordon from 1886 until 1908, by Ben Erbe from 1908 until 1911, by Teman Gordon (and later his estate) from 1911 until at least 1940, by Roy Fink from at least 1945 until at least 1955, by the American Legion in 1960, by James Scott in 1965, by Zwald Properties in 1970, and by William and Muriel Olson since at least 1975. The Granum/Gordon House is not individually eligible for historic designation, and is a noncontributing element in the potential locally-designated Main Street District.

Further, Section 17.17 is the historic preservation portion of the Zoning Ordinance. This ordinance establishes the Historic Preservation Commission to oversee locally-designated historic structures. Today, there are no locally-designated historic structures or sites in Mount Horeb. The subject properties and proposed project do not require conformance with Section 17.17.

PDD Zoning District

A Planned Development is a unique zoning district which is specific to a particular project or area. In addition to enabling flexibilities from zoning requirements, Planned Developments also enable the imposition of higher standards for design and operation, and/or requirements related to timing. With the resulting combination of customized flexibility and control, every Planned Development is reviewed on its relation to the subject property, nearby properties, and the community as a whole.

Every Planned Development has three steps:

- Conceptual discussion to identify project concepts and concerns.
- General Development Plan (GDP) to establish the unique zoning district.
- Specific Implementation Plan (SIP) to approve design and operation details.

If the GDP is approved, the approval of a Specific Implementation Plan (SIP) will be required. The SIP submittal requires all of the details of development, including specific exterior materials and colors, floor plans, detailed exterior lighting plans, and detailed landscaping plans.

Section 17.49 includes the purpose statement of the Planned Development District. It states:

“The purpose of this section is to provide regulations that govern the procedure and requirements for the review and approval, or denial, of proposed planned developments, and to provide for the possible relaxation of certain development standards pertaining to the underlying standard zoning district. The regulatory framework for planned developments is designed to encourage and promote environmental quality by allowing for greater freedom, imagination and flexibility in the development of land while ensuring substantial compliance to the basic intent of this chapter and the Comprehensive Plan.

Planned developments are intended to provide incentives for development and redevelopment in areas of the community that are experiencing a lack of significant investment. Furthermore, planned developments are designed to forward both the aesthetic and economic development objectives of the Village by controlling the site design and the appearance, density or intensity of development in terms of more flexible requirements for land uses, density, intensity, bulk, landscaping, and parking requirements. In exchange for such flexibility, the Planned Development

shall provide a much higher level of site design, architectural control and other aspects of aesthetic and functional excellence than normally required for other developments.

Planned Developments have the potential to create undesirable impacts on nearby properties if allowed to develop simply under the general requirements of this chapter. In addition to such potential, Planned Developments also have the potential to create undesirable impacts on nearby properties which potentially cannot be determined except with a binding site plan, landscape plan and architectural plan, and on a case-by-case basis. In order to prevent this from occurring, all Planned Developments are required to meet certain procedural requirements applicable only to Planned Developments, in addition to the general requirements of this chapter. A public hearing process is required to review a request for a Planned Development. This process shall essentially combine the process for a Zoning Map amendment with that required for a conditional use, with several additional requirements.”

Through the Planned Development process, the project must follow the required process steps, and the Plan Commission must determine if the development warrants greater freedom, imagination and flexibility, while still being substantially compliant with the basic intent of the Zoning Ordinance and [Comprehensive Plan](#). The Plan Commission has determined this to be the case for more than ten projects in the past five years. One of the driving factors for the Zoning Ordinance rewrite process was to reduce the frequency and consistent need for utilizing the Planned Development process.

Other applicable Planned Development District standards include:

- Permitted within all zoning districts
- No minimum area requirement for redevelopment or infill projects
- Flexibilities may be granted from the underlying zoning district associated with land use, density and intensity, bulk dimensional standards, landscaping, signage, parking and loading, and lot, block, and street configuration requirements.
- Flexibilities must be identified within the General Development Plan (GDP)

PDD Criteria For Approval

The following criteria for approval are outlined within the PDD Section 17.49 (analysis provided with underlined text):

- Character and intensity of land use. In a Planned Development District, the uses proposed and their intensity and arrangement on the site shall be of a visual and functional character, which:
 - Is compatible with the physical nature of the site with particular concern for preservation of natural features and open space.
 - The subject properties include two structures and a vacant lot. Dating back to the 2017 Downtown Plan, it has been envisioned that the vacant lot would be redeveloped. Additionally, the subject properties are surrounded by existing development. This block of East Main Street and its counterpart to the south includes a transition of development patterns today from zero lot line, fully built out mixed-use buildings to the west and former residential structures that have been converted into commercial uses or mixed-use buildings to the east. The proposed project would create a uniform street edge along East Main Street and wrap that around a portion of North 3rd Street. The proposed building’s East Main Street façade is broken up visually utilizing articulation to appear as multiple smaller

buildings and portions of the façade employ a setback to give the visual appearance of a shorter building from the street and alley.

- Produces an attractive environment of sustained aesthetic and ecologic desirability, economic stability and functional practicality and complies with the Master Plan, including any residential density limitations, for the area as established by the Village.
 - As discussed above, the proposed project would require flexibility from the Zoning Ordinance’s standards for dwelling unit density. As discussed below, the proposed project can be viewed as consistent with the Comprehensive Plan, but that will ultimately be a determination made by the Plan Commission. There are no residential density limitations within the Comprehensive Plan for this area as it was envisioned that any redevelopment would require some flexibility related to what could be developed on a given a site.
- Will not adversely affect the anticipated provision for school or other municipal services.
 - The subject properties are currently served by municipal services. See the Village Engineer’s Report for more information on traffic and stormwater. It is anticipated that very few, if any, school-aged children will reside in the project given that 34 of the proposed 44 units will be studio or one-bedroom apartments. Based on Village staff discussions with the School District, it is unlikely that any adverse impacts will be created for the area schools.
- Will not create a traffic or parking demand incompatible with the existing or proposed facilities to serve it.
 - See the Village Engineer’s Report for more information on traffic impacts. As discussed above, the Downtown Parking Study identifies capacity to support the proposed project’s residential visitors and commercial patrons. The residential in-building parking standards must be determined by the Plan Commission.
- Economic feasibility and impact. The petitioner shall provide evidence satisfactory to the Village Board that the proposed development is economically feasible, that the developer has adequate available financing, and that the development will not adversely affect the economic prosperity of the Village or the values of surrounding properties.
 - The Village’s economic development consultant prepared an Economic Impact Analysis of the proposed project in July 2025. It found that the initial construction of the \$20 million dollar project will have direct and indirect employment impacts and gross outputs. Further, it found that the potential assessed value following construction could be upwards of \$15,400,000, yielding nearly \$250,000 in property taxes each year. Today, the subject properties have a combined assessed value of \$714,700 and generate \$11,428 in property taxes annually. Finally, it found that the potential new residents that live within the proposed project could generate a significant economic boost in new spending within the downtown area to support existing and new local businesses. To note, this analysis was based on the size of the previous proposal, and some figures may be subject to change with the proposed project as it is presented today.

- The applicant has developed several other projects within Mount Horeb and across Dane County. The Village Board will ultimately have to make the determination on the economic feasibility of the developer and their financing.
- Engineering design standards. The width of street rights-of-way, width and location of street or other paving, outdoor lighting, location of sewer and water lines, provision for stormwater drainage, or other similar environmental engineering considerations shall be based on determinations approved by the Village Engineer as to the appropriate standards necessary to implement the specific function in the specific situation; provided, however, in no case shall such standards be less than those necessary to ensure the public safety and welfare as determined by the Village.
 - See the Village Engineer's Report for more information on traffic impacts.
- Preservation and maintenance of open space. In a Planned Development District, not less than 15% of the total land area of such district shall be designated as open space and adequate provision shall be made for the permanent preservation and maintenance of such open space either by private reservation or dedication to the public. In the case of private reservation, areas of open space shall be protected against building development by conveying to the Village, as a condition for project approval, an open space easement over such open areas restricting any future building or use except as is consistent with such easement.
 - Provision shall be made to landscape open space for the aesthetic and recreational benefit of the development.
 - The care and maintenance of such privately reserved open space shall be assured by establishment of appropriate management organization. The manner of assuring maintenances shall be included in the title to each property in the development.
 - The Village has approved multiple PDDs within the Central Business District and haven't required a minimum of 15% open space (Duluth Trading Company Corporate Headquarters, Front Street Station, Hoff Building). This standard makes sense when considering a greenfield project that typically has additional space within the site to accommodate such open spaces. I believe the intent of this provision was to ensure that greenfield development projects fit the context of the development patterns in the area. A downtown redevelopment project is distinctly different than that. It would not fit the context of the existing development patterns in the area to require open spaces, nor would it be possible in many circumstances given site constraints experienced with most redevelopment projects. This requirement would also be counter to the Zoning Ordinance's purpose statement and standards within the Central Business District, which require zero setbacks and full use of the property.

Traffic and Stormwater

Please refer to the Village Engineer's Report on these items.

Comprehensive Plan

The Village's 2022 Comprehensive Plan designates the subject properties in the Central Mixed Use land use category which is defined as "intended for pedestrian-oriented commercial, office, community facility, and upper story residential uses in a "downtown" setting, with on-street parking and minimal building setbacks. The Central Mixed-Use land use category includes the historic downtown area. The existing Village zoning district that is most appropriate to implement this future land use category is the Central Business District (CB)."

Further, the Comprehensive Plan indicates a set of policies and programs for the Central Mixed Use land use category, including:

- Continue to implement the recommendations of the Downtown Redevelopment Plan.
- Promote downtown as a tourist, commercial, civic, and social center.
- Work with downtown property owners and businesses to preserve, renovate, modernize, and put to active use historically significant buildings.
- Support the expansion, retention, and upgrading of specialty retail, restaurants, financial services, offices, neighborhood retail and services, and community uses.
- Require that all projects submit and have approved detailed building elevations and site plans, showing the proposed locations of the building(s), parking, storage, loading, signage, landscaping, and lighting prior to development approval.
- Protect the unique quality of downtown by requiring buildings to be at least two stories with a zero-lot line front setback.
- Promote truly mixed-use development with residential above the first floor and design oriented to pedestrian and not automobiles.
- Consider rewriting the Village Zoning Ordinance to establish permitted by-right mixed-use land uses in the Central Business District, potentially increasing maximum height for buildings to 72 feet for flat roofs and 85 feet with pitched roof with stepback requirements for any building exceeding four stories or 50 feet in height, and improve the historic preservation standards in this district.

Other relevant goals, objectives, and policies from the plan that align with the proposed development include the following:

- Support mixed use development projects as designated on the Future Land Use Map that integrate non-residential and residential uses into high-quality, unified places.
- Identify opportunities for and actively encourage infill and redevelopment opportunities in areas where existing utilities and infrastructure are available.
- Provide a variety of housing types, densities, arrangements, and costs to accommodate the needs, desires, and life cycles of existing and future residents.
- Promote infill and redevelopment practices to help diversify the community's housing supply.
- Guide new housing to areas within the Village with convenient access to commercial and recreational facilities, transportation systems, schools, shopping, jobs, and other necessary facilities and services.
- Encourage residential uses be incorporated into upper-story downtown buildings, infill, redevelopment, and new commercial/office development to increase the viability and vitality of the community.

The Village's overarching vision statement states: "In 2042, the Village of Mount Horeb will continue to be a thriving and active community that builds on its unique culture, character, assets, location, and history to foster community, economic, and tourism development. The Village will continue to grow and evolve by developing and retaining local businesses, attracting new businesses and employers, and offering housing availability and affordability, while maintaining its character, aesthetic look, and feel. A vibrant downtown featuring the community's historic past and bright future acts as the community's center piece for cultural, recreational, business, shopping, and tourism activities. Building on its high quality of life, the Village will be a welcoming, safe, clean, healthy, and inclusive place to live, work, and play."

Additionally, the plan's overarching goals include:

- Grow responsibly through well-planned, orderly, coordinated, and high-quality development, while maintaining the community's character, look, and feel.
- Promote a future land use pattern containing a mix of uses and building types, while respecting the Village's historic character.
- Encourage infill and redevelopment within the Village, while also providing opportunities for new housing, business, and industrial development along the periphery.
- Protect and preserve the important natural resources in and around the Village.
- Increase the types and affordability levels in the Village's housing stock.
- Emphasize intergovernmental communications and cooperation in planning efforts.
- Enhance the sense of community and recreational opportunities within the Village.
- Ensure the provision of high-quality services and excellent schools.

The Plan Commission will have to determine if it believes that the proposed project is consistent with the adopted Comprehensive Plan. There are many components of the proposed project that align with the plan's land use category, goals, objectives, and strategies, vision, and overarching goals. However, the idea of preserving and maintaining the Village's community character, aesthetic look, and feel are inherently subjective and can have a different meaning to each individual based on their viewpoint. The plan does not define what Mount Horeb's community character is, but does provide terms gathered during the process that residents used to describe it:

- Tight knit community where you run into people you know on the street
- Attractive and aesthetically unique, not a suburban feel
- Small-town feel with cultural connection and sense of place
- Walkable
- Historic and active downtown
- Community is built around the schools
- Civic engagement
- Significant natural resource and recreational opportunities
- High quality of life

As the primary author of the Comprehensive Plan, I believe that the intent of the plan was to enable and promote redevelopment, create new housing and commercial opportunities, grow the local tax base, foster new high-quality mixed-use development, encourage walkability, and grow the local economy.

[Downtown Plan](#)

In 2017, the Village adopted the Downtown Redevelopment Plan. The plan's vision statement states that the Village will "Create a healthy mix of downtown businesses and residences (destination retail, food, beverage, service, entertainment, and single/multi-family housing) that will serve as a magnet for local residents and tourists/travelers."

The plan identified key themes that are the focal points in supporting growth of businesses, branding, marketing, and promotion. Those themes include trails, outdoor recreation, destination shopping, history/heritage, local food and agriculture, arts and artisans, indoor programming entertainment, and residential housing opportunities. Several of these align with the proposed project, including commercial space that could feature any number of new businesses fitting one or more of these themes, creating new

residential housing opportunities, and featuring design themes on the building's facade that speaks to the Village history and heritage.

Additionally, the identification of redevelopment opportunities was analyzed within the plan. While the plan does not identify all three of the subject properties for redevelopment, it does identify the currently vacant property directly adjacent to the MHTC building. The property is shown as a short-term redevelopment opportunity, though few other details are provided. Furthermore, within Map 3 ([Downtown Plan – Proposed Land Use](#)), there are three different proposed land uses shown along the block. This includes Government and Institutional for the MHTC building, Historic Downtown/Central Business for the currently vacant parcel, and Main Street Business for the two eastern parcels that currently have structures on them today. This does not exactly match what is shown within the [Comprehensive Plan's Future Land Use Map](#) that was updated in 2022 to designate all properties along the block as Central Mixed Use.

Formal General Development Plan (GDP) Review

Requested Flexibility from Zoning Ordinance Requirements:

The proposed GDP identifies the following items of flexibility from the standards of the Central Business (CB) zoning district:

1. The **Permitted Principal Land Use** established for the CB district in Section 17.43(2) of the Zoning Ordinance permits Commercial Apartments only as an accessory land use. Multi-Family is proposed to be one of the principal land uses for this project and needs to become a permitted principal land use. The proposed use contains two principal land uses: multi-family (Commercial Apartments) and commercial (specific business types to be determined).
2. The **Maximum Number of Dwelling Units Limit** established for Multi-Family uses in Section 17.20(1)(a)7. of the Zoning Ordinance will not be met by this proposal. Specifically, the maximum number of dwelling units per building is 8 for Multi-Family land uses. The proposed use is 44 units in one building.

Analysis of Requested Flexibilities

The project's proposed flexibilities are needed because of the existing Zoning Ordinance's stringent requirements for dwelling unit density and lack of modern mixed-use development standards.

While the Zoning Ordinance is currently being rewritten to address these specific types of situations and foster new housing and mixed-use opportunities, today the ordinance has stringent requirements on the number of units per building and structure of mixed-use buildings and land uses.

The project is also in direct response to the [Village's 2021 Housing Report](#) commissioned by the Housing Task Force. The report identified that there are projected increases in new households over the next decade and a need for more housing options. It also surveyed residents, and over 50% of respondents stated that there weren't enough rental options in the Village today.

In 2025, the Village completed a [Housing Assessment](#) to better understand the current market considering changes that have occurred since 2021. Some of the key takeaways from the assessment include:

- One-unit attached and one-unit detached housing makes up 85% of the Village's housing stock

- The current overall vacancy rate in Mount Horeb is 3.1%
- There is significant demand for new housing units in Mount Horeb (between 435-935 new units)
- Two of the largest age cohorts in the Village are residents age 15-24 and 55-64
- Both employment and labor have grown by approximately 35,000 over the last ten years in Dane County

In 2024, Dane County completed the [Regional Housing Strategy](#). This included an analysis of the existing and projected housing situation, but also identified strategies, policies, and actions that could be taken to combat the existing housing situation. Below are several key data takeaways from that report.

Documented Housing Shortage:

- Dane County added 42,000 new households between 2010-2020, making it the fastest growing county in the state. However, only 34,000 new housing units were constructed during this time period.
- The number of jobs in Dane County grew by 14% between 2010-2021, adding over 38,000 new employment opportunities, which is more than double the rate experienced across the state during that time.
- Dane County's rental vacancy rate in 2020 was 3 percentage points below the national average and average rents have increased by 28% between 2010-2022.
- The fastest growing age groups within the Dane County between 2010-2020 included young adults (age 15-24) at 15% and retirees (age 55-74) at 31%.
- Homeownership rates across Dane County have decreased from 65% in 2010 to 59% in 2021.
- Between 2020-2040, Dane County is projected to add over 89,000 net new households or a 38% increase. That's almost 4,500 new households per year.
- Between 2020-2040, Mount Horeb is projected to add 1,831 new households and need 103 new housing units per year to keep up with projected demand.
- The 2024 Dane County Housing Strategy sets a goal of constructing 7,000 new housing units each year over the next 20 years to address forecasted household growth.

While the Village has approved several new housing projects within the last year, housing construction is not keeping pace with demand. This is a trend experienced across Dane County. [The Regional Data Group recently released population projections for Dane County](#). The Capital Area Regional Planning Commission worked in partnership with the Madison Metropolitan Planning Organization, Dane County, City of Madison, and University of Wisconsin to produce the new projections (Regional Data Group). The analysis found that Dane County's current population (611,419) is projected to increase to 887,000 over the next 25 years (2050) or an average of 11,000 new residents per year. Specifically related to Mount Horeb, the projections indicate a potential of 1,034 new residents by 2030, 2,424 new residents by 2040, and 3,994 new residents by 2050. The Village of Mount Horeb's 2025 population is 8,006 according to the [Wisconsin Department of Administration](#).

For further context, the Village's [2022 Comprehensive Plan](#) projected an increase of 1,765 new residents between 2020-2040, which is about 660 fewer than what these new projections predict.

To help understand the housing situation in Dane County, I reached out to planners across Dane County that work for municipalities ranging in population from 8,000-13,000. Below is a summary of the number of new units permitted between 2020-2025 within each community that responded.

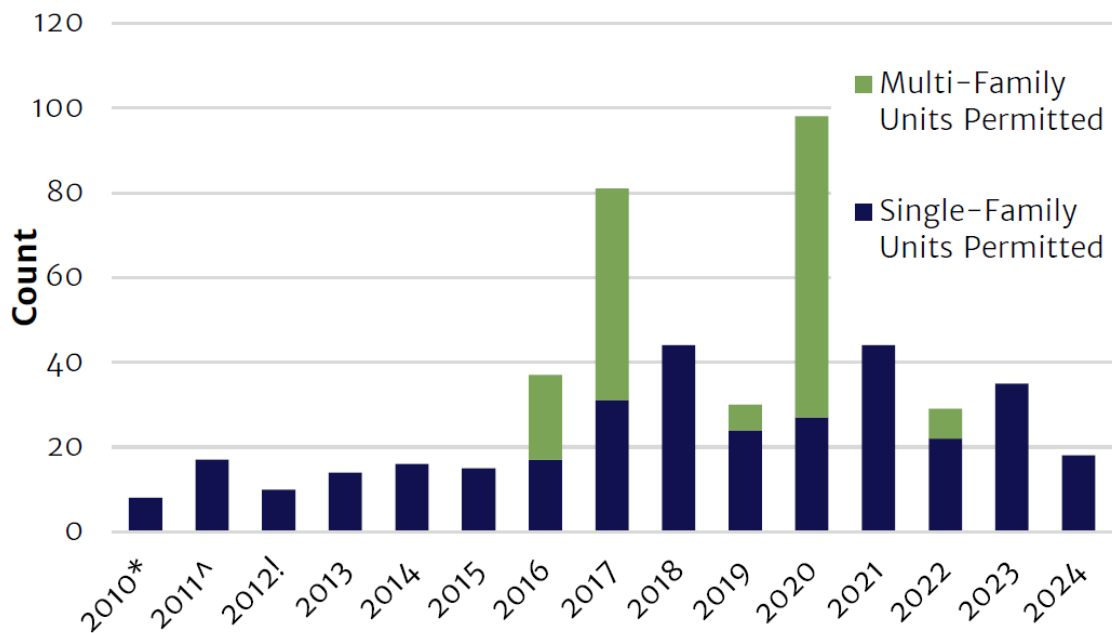
Municipal Population Comparison

Municipality	2025 Population*
Village of Mount Horeb	8,006
Village of McFarland	9,737
Village of Windsor	10,472
Village of DeForest	13,101
City of Monona	9,084
Village of Cottage Grove	9,470

*Source: Wisconsin Department of Administration, 2025.

Mount Horeb Unit Construction Trends ([2025 Housing Assessment](#))

Figure 18. Unit Construction Trends



Number of Housing Units Permitted

Municipality	2020	2021	2022	2023	2024	2025 (to date)
McFarland	152	127	59	25	34	31
Windsor	79	204	222	142	150	No data
DeForest	64	241	126	244	443	No data
Monona	145	0	45	173	300	282
Cottage Grove	55	567	37	68	41	144

Source: Village of McFarland, Village of Windsor, Village of DeForest, City of Monona, Village of Cottage Grove.

The documented need for more housing and greater housing diversity in the Village is one reason that additional mixed-use and multi-family development is needed within Mount Horeb.

Additionally, as described throughout this staff report, the project can be viewed as being in alignment with the [Comprehensive Plan](#) and [Downtown Plan](#), not adversely impacting the downtown parking or traffic situation as illustrated by the [Downtown Parking Study](#) and Traffic Impact Analysis, providing the ability to generate significant economic impact as illustrated within the Economic Impact Analysis, and helping to address the Village's and County's housing shortage as documented within [Village's 2021 Housing Report](#), [2025 Housing Assessment](#), [2022 Comprehensive Plan](#), [2025 Dane County population projections](#), and the [2024 Dane County Regional Housing Strategy](#).

In my professional opinion, this project provides great benefits to the community, is supported by data, facts, findings, and studies, aligns with community plans and urban planning best practices, and addresses community challenges.

Additional Review

The following are observations of the proposed project, many of which will be further reviewed through the SIP stage.

1. **Building Quality:** The proposed GDP provides a high-quality building of sound design and materials that are reflective of the aesthetic look of the area. In particular, the façade is visually broken up to appear as though its four separate structures, utilizes a setback along portions of the façade to create a visual appearance of a smaller structure, creates a uniform street edge along East Main Street on this block that is reflective of those blocks to the west, and promotes walkability and street activity.
2. **Project Layout:** The project layout works well within the confines of the existing development patterns surrounding it. The setbacks are reflective of buildings directly to the west, vehicle access is distributed between the rear alley and North 3rd Street offering opportunities to utilize both the North 2nd Street/Main Street intersection and North 3rd Street/Main Street intersection for ingress/egress, residential tenant parking is provided within the building, and pedestrian access is provided along East Main Street and North 3rd Street. The commercial portion of the proposed project directly fronts onto East Main Street, providing a more engaging and active street frontage than today.
3. **Project Scale/Number of Units:** The proposed GDP represents a density of 44 dwelling units on 0.73 acres – or about 60 dwelling units per acre. This is lower density in comparison to other previously approved projects downtown for the Hoff Building (100 units/acre) and Front Street Station project (85 units/acre). The inclusion of 9,164 square feet of commercial space approximately doubles the amount of commercial space provided on the properties in comparison to today (4,600 square feet existing).

Overall, the building scale, while larger than that of the existing structures on-site and buildings to the east and north, is reflective of other structures in the downtown area. There is no denying that this would be a change in building size and scale from what's on the site today, in addition to being larger than the existing MHTC building to remain to the west. However, the reduction of the building height down to three stories is more keeping with the heights of the Village Hall and Opera House buildings directly to the west of the subject properties along Main Street. Additionally, the design aims to create a visual appearance from the street of a smaller structure utilizing variation in façade materials, recesses, projections, and setbacks.

4. **Architecture:** The proposed GDP as depicted on the elevations has a flat roof which fits well in the context of the existing development patterns to the west, but is different than that of the structures

that exist to the east. Overall, variation in materials, façade heights, projections within the façade, glazing, and unit porches provide articulation along each façade.

5. **Site Design:** The proposed GDP as depicted on the site plan provides vehicle, pedestrian, and bicycle access to the project and parking for all modes. See the Village Engineer's report for more information on traffic safety impacts.
6. **Landscaping:** The proposed GDP does not include landscaping and this is reflective of the downtown development pattern and Zoning Ordinance requirements.
7. **Lighting:** The proposed GDP does not include an exterior lighting plan. The specific lighting plan for the site will be reviewed as part of the SIP process.
8. **Signage:** The GDP proposes on-building wall signs for the building and future commercial tenants. The specific signage plan for the site may be submitted and reviewed as part of the SIP process.
9. **Requested Zoning Flexibility:** I support granting flexibility for the use and density requirements because of the following. First, the Zoning Ordinance standards are clearly outdated and are currently being rewritten to address the existing ordinance's deficiencies. Second, the project helps meet the housing shortage currently in the Village and increases the diversity of the Village's housing stock. Third, one of the core goals of the [2017 Downtown Plan](#) was to increase housing options within downtown. Fourth, the new mixed-use structure provides new commercial spaces directly located along East Main Street that will help activate and enhance the streetscape. Finally, I believe that the developer took the feedback from the October public hearing and made modifications to the plan set to address many of the community's concerns including: building size, density, height, parking, and traffic. While understandably a big change from what's on the site today, the proposed development provides a unique and high-quality mixed-use redevelopment within downtown Mount Horeb.

Village Planner's Recommendations Regarding the GDP

If acceptable to the Plan Commission, the Village Planner recommends the Plan Commission make, consider, and adopt a motion which makes a finding that the criteria for GDP submittals and approval have been met, and that also recommends approval of the proposed GDP Zoning Map Amendment by the Village Board, inclusive of full and continuous compliance with the submittal documents, the list of flexibilities and conditions provided in the submittal, and with any conditions identified by Village Staff and the Plan Commission, including all of those identified below:

1. A Plan Commission determination that the proposed GDP is consistent with the Village's adopted Comprehensive Plan and the purpose statement of the PDD Zoning District as required in Section 17.49(1).
2. A Plan Commission determination that the proposed GDP is consistent with the PDD criteria for approval in Section 17.49, with the exception of the required 15% open space that has been waived as part of past PDDs within the downtown area and would be waived for the proposed project.
3. A Plan Commission determination that as an essential component of the site's design per Section 17.43(7), the maximum permitted setbacks of 0 feet within the CB Zoning District may be exceeded as depicted on the GDP.
4. A Plan Commission determination that the minimum vehicle parking standards of 1.25 spaces per dwelling unit be applied to all residential units within the structure and no on-site commercial parking is required.
5. The proposed flexibility to allow Commercial Apartments as a principal by-right land use in the CB zoning district should be granted as proposed in the GDP submittal. All other principal, accessory, and temporary land uses within the CB zoning district shall remain the same.
6. The proposed flexibility to allow the maximum dwelling units per building to exceed 8 dwelling units per building should be granted up to 44 dwelling units as proposed in the GDP submittal.
7. The approval of the Certified Survey Map as depicted, contingent upon Village Board approval of the GDP.
8. The Village's design review standards be applied through the SIP phase of project review.
9. Any additional requirements identified by Village Staff, the Village Engineer, the Plan Commission, or the Village Board.



VANDEWALLE & ASSOCIATES INC.

MEMORANDUM

To: Village of Mount Horeb Plan Commission
From: Village Planner: Ben Rohr, AICP
Date: November 19, 2025
Re: Specific Implementation Plan (SIP) and Design Review for a Proposed 3-Story Mixed-Use Building at 210, 214, and 220 E. Main Street for Jeff Grundahl.

Introduction

Jeff Grundahl has submitted the Specific Implementation Plan (SIP) for 210, 214, and 220 E. Main Street for Plan Commission review. The project is proposed to be located directly east of the Mount Horeb Telephone Company building on the block bounded by East Main Street, North 3rd Street, North 2nd Street, and the public alley. This block and its counterpart on the southern side of East Main Street serve as a transitional area of downtown development patterns. This includes blocks to the west featuring zero setback buildings and contiguous block faces that transition to the east to feature mixed setback former residential structures that have been converted into commercial businesses or mixed-use buildings. North of the subject properties and public alley also features a transition of development patterns into mixed residential neighborhoods and off-street parking lots.

This project is also seeking General Development Plan (GDP) zoning approval at the November meeting. To receive full Village approval of the Planned Development, the project requires approval of the SIP and Design Review by the Plan Commission and the Village Board. This report addresses the SIP first, followed by the Design Review.

Specific Implementation Plan Review

Jeff Grundahl and his design team have been working with the public, Village Staff, and the Plan Commission to refine their design over the past several months through multiple rounds of meeting discussions.

In order for the Planned Development to be approved, the SIP must meet the zoning requirements, inclusive of the zoning flexibilities in the GDP.

Specific SIP Flexibilities Consistent with the GDP (if approved) include:

1. Allowable Land Uses: The proposed GDP identifies multiple by-right principal land uses for the property – Commercial Apartments (multi-family) and Commercial (specific business types to be determined). The SIP is consistent with the proposed GDP.

2. Number of Dwelling Units in Building: The proposed GDP includes a building with up to 44 dwelling units. The SIP is consistent with the proposed GDP.

Planner's Recommendation for the Specific Implementation Plan:

Pending Plan Commission action on the GDP, I recommend the Plan Commission recommend the SIP for 210, 214, and 220 E. Main Street for approval by the Village Board, subject to the waivers and requirements addressed at the end of this report, as discussed and approved by the Plan Commission.

Design Review Requirements per Section 17-14:

Design Review by the Plan Commission is required under Section 17.14 of the Zoning Ordinance. Design Review is required for all development projects in the Village, except residential development with three or fewer dwelling units. The Design Review requirements in Section 17.14(4)(b) identify 27 Design Directives. The Plan Commission has the ability to waive any Design Directive or add additional design requirements upon a finding that the additional design requirement would improve the project.

This review is based on the plan set provided by the applicant. This project complies with most Design Review requirements. Any items that do not comply with the Design Directives or requires Plan Commission discussion have been identified with underlined text.

1. Site Grading:

- a. The grading plan shall preserve existing grades to the greatest extent possible, particularly at the perimeter of the site.
 - This requirement is met. The Grading Plan works within the existing grade changes on-site today and maintains the site's topography to the extent possible.
- b. In no instance shall proposed grading create transitions to adjoining properties or rights-of-way that are unsafe, unstable or create drainage problems.
 - This requirement is met. Existing grade transitions to adjacent properties are maintained to the extent possible.
- c. Site grading shall also preserve or create an attractive site appearance in conjunction with the proposed development.
 - This requirement is met. Existing site grades are maintained to the extent possible.

2. Existing Site Vegetation and Other Natural Features:

- a. The grading and development plans shall preserve, protect, and maintain important existing site vegetation as identified by the Plan Commission. Examples include hedgerows, groves and individual specimen trees.
 - There is existing vegetation along the northern property lines (alley), landscaping around the existing buildings, and arbor vitae adjacent to the MHTC building that are proposed to be removed to facilitate the project. There are two larger trees in the right-of-way along Main Street and one along 3rd Street. The developer intends to use an arborist to assist in taking measures to preserve each during the construction process. I recommend that this requirement be waived to accommodate the proposed project because the existing on-site vegetation cannot be saved if the building occupies nearly 100% of the parcel and the developer is taking measures to preserve the larger trees in the adjacent right-of-way.
- b. Topsoil used at the development site should be from on site, to the extent possible, particularly in areas under the drip line of preserved vegetation.

- This requirement is met. The proposed development works within the site's existing topography to limit grading impacts to the extent possible.

3. Site Layout Relation to Abutting Streets:

- In general, the layout of buildings, structures, pavement, parking and landscaping shall be compatible with the general pattern for similar land uses in the vicinity, as determined by the Plan Commission.
 - This requirement is met. The project is generally reflective of the development patterns to the west along Main Street, including the zero setbacks, flat roof, mixed-use design with commercial below and residential above, ingress off of 3rd Street and egress on the rear alleyway, in-building parking, and a fully built-out site design. The existing topography on the site creates a situation where the building will be taller along Main Street because of the lower grade and shorter along the rear alleyway because of the higher grade.
- The general site layout shall result in an attractive appearance to all abutting streets, as determined by the Plan Commission.
 - This requirement is met. Same as 3.a., above.

4. Building Setbacks:

- Building setbacks shall comply with the requirements of the zoning district, and with the additional requirements listed below in subparagraphs 4.b. and 4.c.
 - This requirement is met if the GDP is approved and the Plan Commission determines that as an essential component of the site's design, the maximum permitted setbacks of 0 feet within the CB Zoning District may be exceeded. All other building setbacks are met.
- Within the downtown area, and particularly along Main Street and the commercially developed portions of intersecting streets, the entire street side of buildings shall be located at or within 3 feet of all abutting street rights-of-way, unless an exception is granted through the conditional use process.
 - This requirement is met if the GDP is approved and the Plan Commission determines that as an essential component of the site's design, the maximum permitted setbacks of 0 feet within the CB Zoning District may be exceeded. All other building setbacks are met.
- Within the CTH ID corridor between STH 78 and USH 151, the entire street side of buildings shall be located at or within 30 feet of all abutting street rights-of-way, unless an exception is granted through the conditional use process. Parking shall not be permitted in this area. Drives for one-way circulation and/or emergency vehicle access may be permitted through the conditional use process.
 - This requirement is not applicable, as the subject property is not located in the specified area.

5. Building Height:

- Building heights shall comply with the requirements of the zoning district, and with the additional requirements listed below in subparagraphs 5.b. and 5.c.
 - This requirement is met. The proposed project has a maximum height of 41 feet and the maximum height permitted within the Central Business Zoning District is 50 feet.
- Within the downtown area, and particularly along Main Street and the commercially developed portions of intersecting streets, building heights shall be a minimum of 20 feet

and shall be a minimum of 2 stories, unless an exception is granted through the conditional use process.

- This requirement is met. The proposed project is 41 feet and three stories along both the Main Street and 3rd Street frontages.
- c. Within the CTH ID corridor between STH 78 and USH 151, building heights shall provide a functional or decorative building tower or related architectural feature that exceeds the height of the majority of the building facade by at least 7 feet, unless an exception is granted through the conditional use process. Roofs, and roof appurtenances such as cupolas, shall not count toward this requirement.
- This requirement is not applicable, as the subject property is not located in the specified area.

6. Building Mass:

- a. Buildings over 5,000 square feet of gross floor area shall be subject to the requirements for large developments.
- The project is subject to the large development requirements. This includes high-quality building materials and design, clearly defined and visible entryways, and nonreflective, natural, and earth tone building colors (outside of accent elements). This requirement is met as described in items 7, 8, and 9 below.

7. Building Facades:

- a. Long building facades shall be visually broken up and variegated with staggers and offsets as determined appropriate by the Plan Commission.
- This requirement is met by regularly spaced building projections and recessions, stepbacks, changes in building façade materials, patios, balconies, and differentiated window sizes along all facades.
- b. All building facades facing streets, drives or parking areas shall provide regularly spaced openings for windows and doors, and regularly spaced decorative elements such as piers and columns. The size and spacing of these features shall be compatible with nearby structures that meet this requirement, as determined by the Plan Commission.
- This requirement is met through regularly spaced openings for windows and doors along each façade, in addition to a front façade along Main Street that utilizes architectural elements to appear as though its four smaller structures that are generally reflective of other building facades further west along Main Street. To note, portions of the ground floor façade along 3rd Street and alleyway do not include glass windows due to the in-building parking location, but does integrate recesses in the masonry façade with rosemaling to accommodate a surface-mounted public art feature.
- c. All building facades facing streets, drives or parking areas shall provide a pattern of upper story openings that is compatible with the pattern established by the ground floor, as determined by the Plan Commission.
- This requirement is met with regularly spaced windows on the upper floors of all facades. To note, there are no upper story windows proposed on a portion of the western façade because that will directly abut the existing MHTC building. However, two recessed dwelling unit porches are included within this corner of the façade.

- d. Within the downtown zoning district, and particularly along Main Street and the commercially developed portions of intersecting streets, building facades shall employ traditional storefront design by providing pilasters, transom windows, cornices, lintels and related decorative details to highlight transitions between the ground floor and upper stories and between upper stories and the parapet.
- This requirement is met. Along both the Main Street and 3rd Street facades traditional storefront design has been integrated with differentiated and decorative architectural detailing. To note, a portion of the ground floor façade facing 3rd Street will include a vehicle access garage door and garbage access garage door. This is further discussed in item 10.a. below.

8. Exterior Building Materials:

- a. Exterior materials for multi-family, office, or commercial buildings may not include metal wall panels, concrete panels, plain concrete block, asphalt, or spandrel window panels, unless an exception is granted through the conditional use process.
- This requirement is met. Proposed building materials include masonry, siding, and stone on all four facades.
- b. Hard and durable materials including decorative block, brick or stone, shall be required on all exterior portions of the structure within 40 inches of the adjacent grade.
- This requirement is met. Brick and stone are proposed along the full length of all four facades.

9. Exterior Building Colors:

- a. Exterior colors of all buildings, structures, appurtenances and paving shall be compatible and harmonious with those of existing buildings in the immediate area which conform to these design requirements, as determined by the Plan Commission.
- i. Within the downtown area, the Victorian color palette shall be used, or other colors must be approved by the Plan Commission.
- This requirement is met. The proposed exterior building materials colors include red brick, cream, browns, and greys along the facades.
- ii. Throughout the rest of the Village, primary colors including red, blue, green, yellow, black, and fluorescent colors shall not be permitted. Where an integral part of site design, muted versions of these colors may be used, as approved by the Plan Commission.
- This requirement is met. The proposed exterior building materials colors include red brick, cream, browns, and greys along the facades.
- iii. High gloss paints and other exterior finishes are not permitted.
- This requirement is met on all portions of the building.
- iv. Color patterns shall be used consistently throughout the property, as determined by the Plan Commission.
- This requirement is met on all portions of the building with a consistent use of neutral colors proposed.

10. Exterior Doors:

- a. All exterior doors visible from streets, drives or parking areas shall provide decorative design elements.
 - This requirement is mostly met. All doors on the south façade are proposed to be glass with metal framing (facing Main Street). The exception is along the east and north side of the structure where non-decorative garage doors to access the in-building parking and garbage area are proposed. I recommend waiving this requirement for those doors as has been the practice with several other projects that employ in-building parking.

11. Windows:

- a. Spandrel panels, mirrored glass or heavily-tinted windows are not permitted. Where screening is desired, vertical or horizontal strip blinds may be used.
 - This requirement is met. No such windows are proposed.
- b. Windows may not be obscured by paper or other attached materials on more than 5% of any individual window panel.
 - This requirement is met. No such windows are proposed.

12. Roofs:

- a. Roof forms and materials shall be compatible and harmonious with those of existing buildings in the immediate area which conform to these design requirements, as determined by the Plan Commission.
 - This requirement is met. The proposed flat roof is consistent with the buildings to the west along Main Street. To note, the existing structures to the east and north have pitched roofs because this block is a transitional area between traditional downtown development patterns and residential style structures. A small, pitched roof section has been included along the eastern façade facing 3rd Street.
- b. Metal roofs shall not be used for multifamily, office or commercial buildings, unless an exception is granted through the conditional use process.
 - This requirement is met. A rubber roof is proposed.
- c. Roof forms shall be designed to accommodate the requirements in subparagraphs 5.c. and 7.d., above.
 - This requirement is met.
- d. Rooftop mechanical equipment shall not exceed the elevation of the lowest point of the surrounding building parapet or roof.
 - This requirement may be met. Air conditioning condenser units and a restaurant exhaust system are proposed to be located on the roof. However, a three-foot parapet above the roof exists on each façade that will be utilized to screen the mechanicals. I recommend requiring staff review of the final design for the mechanicals to ensure they are fully screened.

13. Exterior Building Appurtenances:

- a. Building facades shall be uncluttered by the minimal use and careful placement of brackets, wiring, meter boxes, antennae, gutters, downspouts and other appurtenances. Where

necessary, such features shall be colored to blend in, rather than contrast, with the immediately adjacent building exterior.

- This requirement is mostly met. Most of the mechanicals will be within the structure and hidden from public view. Additionally, no downspouts are proposed as drainage will be internal from the roof. However, meter boxes will be required to serve the units and are clustered on the façade of the building in the northwest corner (facing the alleyway). Additionally, air louvers will be required for air circulation for interior mechanicals. I recommend waiving this requirement for the meter boxes and requiring staff review of the final design for the air louvers to ensure they match and are consistent with the building design.

14. Awnings:

- a. Awnings, where approved by the Plan Commission, shall complement the character of the building.
 - This requirement is met. Varying styles of canopies/awnings are proposed along both the Main Street and 3rd Street facades. Each are slightly different in style to provide visual variety associated with making the building appear as though its four smaller buildings. However, they are overall consistent with the color scheme and materials of the building.
- b. Soft, weather-treated canvas or vinyl materials which allow for flexible or fixed installation may be used. Metal canopies are prohibited.
 - This requirement is not met. One awning is proposed to be made of soft, vinyl materials (southwest corner), five of the canopies/awnings are proposed to be metal, and the corner canopy element will employ a cedar shake, pitched roof made out of wood materials. However, the canopy/awnings all match the color scheme and materials of the building. I recommend that this requirement be waived.
- c. Backlighting is prohibited.
 - This requirement is met.

15. Trash Containment Structures:

- a. Trash containment structures shall be designed to fully conceal trash storage.
 - This requirement is met. The trash containment structure is proposed to be located within the first-floor structured parking area (northeast corner).
- b. Trash containment structures shall be constructed in the same materials and colors as the ground floor of the principal building.
 - This requirement is not applicable, as the structure will be located within the building.
- c. Wood or synthetic wood gates shall be used and shall be arranged in a pattern to provide complete opacity. Metal panel gates and metal fence gates with strips are prohibited.
 - This requirement is not applicable, as the structure will be located within the building.

16. Freestanding Canopy Structures:

- a. Freestanding canopy structures, such as those providing shelter at a gas station or drive-up banking station, shall be designed to complement the design of the principal building, as determined by the Plan Commission.
 - This requirement is not applicable. There are no freestanding canopy structures proposed.

- b. Canopy roofs shall employ the same pitch, materials and colors as the roof of the principal building, unless an exception is granted through the conditional use process.
 - This requirement is not applicable. There are no freestanding canopy structures proposed.
- c. Canopy supports shall be decorative in nature and shall employ the same materials and colors as the principal building. Exposed nondecorative supporting poles are not permitted.
 - This requirement is not applicable. There are no freestanding canopy structures proposed.

17. Other Structures:

- a. Outbuildings shall employ the same high-quality exterior building materials as used on the principal building.
 - This requirement is met. No outbuildings are proposed.
- b. Walls and fences shall employ high-quality materials such as metal pickets, stone, decorative block or brick which complement the principal building, as determined by the Plan Commission.
 - This requirement is met. The only fence proposed is located between the existing MHTC building and the new building (1' distance). It's proposed to be 8' tall and made of solid metal. It will be mostly hidden from view as it's proposed to be recessed within the 1' gap between the buildings.

18. Exterior Lighting:

- a. All exterior lighting shall use decorative fixtures, as approved by the Plan Commission. Such fixtures shall be consistent in design theme throughout the site.
 - This requirement is met. Fixtures include can lights under the canopies/awnings, black wall-mounted light packs, and black goose neck mounted lighting.
- b. All exterior light bulbs shall not be visible from beyond any property line. Directional or cutoff fixtures shall be used to meet this requirement.
 - This requirement is met. All fixtures are downward facing and directional in nature. However, as noted in item 18.c. below, lighting does extend beyond the property line.
- c. Lighting at the property lines shall not exceed 0.5 footcandles over ambient conditions.
 - This requirement is not met. The lighting plan indicates that light levels at the property line will exceed 0.5 footcandles on the north, east, and south elevations. Due to the nature of this project, it is not possible to meet this standard because the building is 6 feet or less from each property line. As shown on the lighting plan, lighting levels of 0.5 or greater will extend into the Main Street, 3rd Street, or alleyway right-of-way, but will extend into neighboring private property, with the exception of the MHTC site to the west. I recommend that the Plan Commission discuss this item.
- d. Maximum lighting shall not exceed 10.0 footcandles on the site, except in areas serving in-vehicle uses such as drive-up windows and fueling areas. In such locations, maximum lighting shall not exceed 25.0 footcandles.
 - This requirement is met. All lighting levels are below 10.0 footcandles.
- e. Freestanding light fixtures shall not exceed a total height of 20 feet.
 - This requirement is not applicable. No freestanding light fixtures are proposed.

19. Exterior Signage:

- a. Exterior signage shall complement the design of the overall site, as determined by the Plan Commission.
 - It is unknown if this requirement is met. Signage is indicated on the building, but no signage details have been provided by the applicant at this time.
- b. Freestanding sign supports shall be decorative in nature and shall employ the same materials and colors as the principal building. Exposed nondecorative supporting poles are not permitted.
 - It is unknown if this requirement is met. Signage is indicated on the building, but no signage details have been provided by the applicant at this time.

20. Outdoor Display or Storage:

- a. All outdoor display and/or storage areas must be clearly depicted and labeled on an approved site plan. Said label shall specify the general types of materials to be displayed or stored, including: retail or rental display; retail or rental vehicle display; long-term business vehicle storage; equipment storage; refuse storage; scrap equipment or vehicle storage.
 - This requirement is not met. Outdoor seating areas are shown on the renderings, but not indicated within the Site Plan. To note, any outdoor seating areas within the right-of-way (public sidewalk portion) will require a Sidewalk Café Permit per Section 12.09 of the Municipal Code. I recommend that this item be added to the Site Plan.
- b. Outdoor storage areas shall be adequately screened from view from adjacent properties and streets, drives and parking areas, as determined by the Plan Commission.
 - This requirement is met. No outdoor storage areas are proposed.

21. Pavement Materials:

- a. All traffic circulation, parking and pedestrian areas shall be provided with a hard, durable surface such as concrete, asphalt, pavement blocks or bricks. Gravel is not acceptable.
 - This requirement is met throughout the site.
- b. Durable, porous pavement techniques are encouraged, and the overall coverage of pavement on all sites should be minimized so as to reduce stormwater runoff.
 - This requirement is met. The stormwater management plan will be reviewed by the Village Engineer and be subject to meeting all applicable requirements. Green roofs have been added to several of the canopies along the Main Street and 3rd Street façade as well.

22. Pedestrian Facilities:

- a. Public sidewalks or multipurpose paths shall be required along all street frontages, per Village standards.
 - This requirement is met. Sidewalk connections are proposed to connect to the existing sidewalk along the full length of both Main Street and 3rd Street.
- b. Paved pedestrian connections are required between all street frontage pedestrian facilities and public doorways. Said connections shall be a minimum of 5 feet wide.
 - This requirement is met. See 22.a. above.

- c. Where pedestrian connections cross vehicle circulation areas, a crosswalk clearly delineated by color and/or texture is required.
 - This requirement may be met. Vehicles will have to cross a portion of the sidewalk along 3rd Street to access the in-building parking area and garage pick-up door. However, these doors are proposed to be ingress only meaning that vehicles will be able to see pedestrians using the sidewalk prior to crossing the sidewalk. No additional markings, bollards, or signage is proposed at this time in this area. I recommend that the Plan Commission discuss this item.

23. Traffic Circulation:

- a. Traffic circulation patterns shall be safe and understandable by vehicle operators, as determined by the Plan Commission.
 - This requirement is met. In-building parking ingress is proposed from 3rd Street and egress is proposed out to the alleyway. This will help distribute traffic patterns between different intersections in the area. The one identified potential pedestrian safety conflict is discussed in item 22.c. above.
- b. Traffic patterns which are determined as too complicated by the Plan Commission shall be prohibited. Complications may be identified due to steep grades, inadequate throat depths, offset intersections, too many intersections within a particular area, dangerous and conflicting traffic movements, movements compromised by limited visibility, or restricted turning radii for the types of vehicles likely present on the site.
 - This requirement is met. Per the Village Engineer's report, there are suggested potential right-of-way changes that the Village could integrate to improve overall safety in the area, but the level of service standard impacts as documented within the Traffic Impact Analysis from the proposed development are still within reasonable amounts.
- c. No traffic circulation shall be permitted within 10 feet of a street or drive right-of-way or easement, nor within 5 feet of any other property line, except within the downtown area, or as approved as through the conditional use process.
 - This requirement is not applicable. The proposed project is within the downtown area.
- d. Within the CTH ID corridor between STH 78 and USH 151, refer to subparagraph 4.c. above regarding limitations on drives located adjacent to street rights-of-way.
 - This requirement is not applicable, as the subject property is not located in the specified area.

24. Parking:

- a. All parking areas located within required principal building setback areas shall be visually screened from streets by a continuous landscaped hedge, fence, wall or berm with a minimum height of 40 inches over the elevation of the adjacent portion of the parking lot at time of installation. Gaps in this hedge are permitted for pavement approved as part of the site design.
 - This requirement is met. All parking will be fully screened within the building.
- b. Within the downtown area, and particularly along Main Street and the commercially developed portions of intersecting streets, parking shall not be located along a street frontage unless an exception is granted through the conditional use process.
 - This requirement is met. All parking is proposed within the structure.

- c. Within the CTH ID corridor between STH 78 and USH 151, refer to subparagraph 4.c. above regarding the prohibition of parking adjacent to street rights-of-way.
 - This requirement is not applicable, as the subject property is not located in the specified area.
- d. Bicycle parking shall be provided at a rate of one bicycle space for every 10 vehicle parking spaces up to a total of 20 bicycle spaces.
 - This requirement is met. 44 bicycle parking stalls are proposed.

25. Building Foundation Landscaping:

- a. A minimum 10-foot wide landscaping bed or planter shall be provided along at least 50% of each wall facing a street, drive or parking area, except within the downtown area.
 - This requirement is not applicable. The proposed project is within the downtown area.
- b. This bed or planter shall be located adjacent to or within 10 feet of the building foundation.
 - See 25.a. above.
- c. This bed or planter shall be landscaped appropriately as determined by the Plan Commission.
 - See 25.a. above.

26. Street Frontage Landscaping:

- a. A minimum 10-foot wide landscaped area shall be provided adjacent to the frontage of all streets and drives, except within the downtown area.
 - This requirement is not applicable. The proposed project is within the downtown area. However, planters have been proposed along both street sides of the building.
- b. This area shall be landscaped appropriately as determined by the Plan Commission. At minimum, one canopy-type street tree (maple, oak, hickory, ginkgo, honey locust or similar species) shall be required for every 50 feet of street or drive frontage, except within the downtown area. Said trees shall be a minimum 2-inch to 2.5-inch caliper.
 - See 26.a. above.

27. Parking Lot Landscaping:

- a. Parking lot landscaping shall comply with the requirements of §17.136
 - This requirement is not applicable. No surface parking lot is proposed.
- b. Subsection 24.a., above, shall also apply along street and drive frontages.
 - This requirement is met.
- c. Subsection 24.c., above, shall also apply along CTH ID.
 - This requirement is not applicable, as the subject property is not located in the specified area.

Village Planner's Recommendations:

Pending Plan Commission action on the GDP, I recommend the Plan Commission consider and approve the following requirements for the approval of the Specific Implementation Plan for 210, 214, and 220 E. Main Street:

1. A recommendation to the Village Board that the Specific Implementation Plan for 210, 214, and 220 E. Main Street be approved, subject to any and all requirements identified by the Plan Commission under Design Review.
2. A recommendation to the Village Board that the Site Plan packet be approved, subject to any and all requirements identified by the Plan Commission under Design Review.
3. Design Review as submitted to the Village, including waivers from the Design Review requirements approved by the Plan Commission, including:
 - a. 2.a. for removal of existing on-site vegetation, as depicted with the condition as noted below in item 7.
 - b. 10.a. for the non-decorative garage doors on the north and east façade for the in-building parking entry and garbage pick-up door, as depicted.
 - c. 13.a. for non-decorative exterior meter boxes on the northern building façade, as depicted.
 - d. 14.b. for metal awnings to be permitted on the south and east façades, as depicted.
4. Village staff review and approval of the following that meets all Design Review requirements. If any waivers are requested by the applicant from the Design Review requirements, the proposed plans must be reviewed and approved by the Plan Commission.
 - a. 12.d. Require detailed rooftop mechanical plans with proposed locations and screening provided.
 - b. 13.a. Require detailed air louver plans with proposed locations, materials, and colors depicted that are consistent with the building design.
 - c. 19.a. and b. Require detailed signage plans for all proposed signs.
 - d. 20.a. Require the proposed outdoor seating area to be identified on the Site Plan.
5. Plan Commission Discussion and Action on either (related to requirement 18.c.):
 - a. Finding that the proposed lighting plan is acceptable with lighting levels greater than 0.5 footcandles at the property line.
 - b. Requiring reduced lighting levels as determined by the Plan Commission.
6. Plan Commission Discussion and Action on either (related to requirement 22.c.):
 - a. Finding that the proposed sidewalk crossings along 3rd Street for vehicle ingress is sufficiently safe for pedestrians because the entrances are ingress only and provide an opportunity for drivers to see pedestrians prior to entering the sidewalk area.
 - b. Requiring additional safety measures to be employed in this area (signage, color/material changes, bollards, etc.) as determined by the Plan Commission.
7. The applicant shall employ an arborist during the construction process to preserve the existing trees within the Main Street and 3rd Street right-of-way.
8. The applicant shall obtain a Sidewalk Café Permit subject to all requirements of Section 12.09 of the Village of Mount Horeb Municipal Code.

9. Village Engineer review and approval of the proposed Stormwater Management and Grading Plans.
10. Any additional requirements identified by Village Staff, the Village Engineer, the Plan Commission, or the Village Board.

Letter of Intent

Langhús on Main Mixed Use Residential/Commercial Development 220 East Main Street, Mount Horeb

November 13, 2025

On behalf of the Owner and Developer, Jeff Grundahl, please accept this revised submittal for a combined General Development Plan and Specific Implementation Plan (GDP/SIP) for the proposed mixed-use residential development at 220 East Main Street, Mount Horeb, WI.

Project Team:

Developer:

Jeff Grundahl, (or assigns)
455 S Junction Road
Madison, WI 53719
608-237-1730
jeff@jgdevelopment.com

Architect:

Sketchworks Architecture, LLC
Steve Shulfer, AIA, Partner|CEO
2501 Parmenter Street, suite 300A
Middleton, WI 53562
608-836-7570
sshulfer@sketchworksarch.com

Traffic Analysis

TADI (Traffic Analysis & Design, Inc.)
Don Lee, PE
PO Box 128
Cedarburg, WI 53012
414.517.5014
dlee@tadi-us.com

Surveyor & Civil Engineer:

Wyser Engineering
Wade Wyse P.E.
312 E Main Street
Mount Horeb, WI 53572
608-843-3388
wade.wyse@wyserengineering.com

Property Manager:

The Gallina Companies
Dawn Hendrickson
101 East Main Street, Suite 500
Mount Horeb, WI 53572
608-237-1300
dhendrickson@gallinacos.com

Land / Parcels:

There are three parcels on Main Street, Mount Horeb that are in consideration for this project:

- 208 E Main Street (currently vacant)
 - BOECK'S, CARL SURVEY & PLAT MT HOREB STATION PLAT OF BLK 1-11 BLOCK 6 LOTS 5, 6, 7 & 8
- 214 E Main Street (currently a small commercial property, formerly residential)
 - BOECK'S, CARL SURVEY & PLAT OF MT HOREB STATION PLAT BLOCK 6 LOTS 9, 10 & 11
- 220 E Main Street (currently a small commercial property, formerly residential)
 - BOECK'S, CARL SURVEY & PLAT MT HOREB STATION PLAT OF BLK 1-11 BLOCK 6 LOTS 12, 13 & 14

The developer currently owns the parcels at 214 and 220 E Main Street and has an accepted offer to acquire and develop the parcel at 208 E Main Street.

It is worth noting that the developer has been able to secure a verbal agreement to move and relocate the 214 E. Main Street/former Olsons Flowers to another location outside of town where it will be used for a personal home. At this time it is the intention of the developer to not have to demolish this structure.

Current Zoning assessment and Development needs:

All parcels are currently zoned "CB" Central Business, per the Mount Horeb zoning code.



Standards Per the zoning code:

Building setbacks (for all sides):	zero feet (0').
Maximum building height:	50'-0" (minimum 20'-0")
Parking:	no minimum parking required

(Refer to later pages which refer to Comprehensive Plan recommendations and compliance)

Proposed Development Overview

The proposed development will include a mixed-use building consisting of:

- Residential apartments: 44 units in total
 - A combination of unit sizes per the attached plans
 - Studio Units: 8 units, ave. 570 sf/unit
 - 1 Bedroom Units: 26 units, ave. 830 sf/unit
 - 2 Bedroom Units: 10 units, ave. 1,150 sf/unit
- Commercial leasable space: ~ 9,155 sf +/-

Apartment amenities will be provided and will include package delivery and dog wash. A management office will be provided within the Hoff Mall building, ensuring quality upkeep and customer service.

Parking

The parking accommodations will include 55 total parking spaces on site. All of the stalls will be at grade, secure, indoor parking, accessed from Third Street and will now exit only to alleyway at the north in order to drive traffic to the stoplight. These will be reserved for residents and business tenants of the building.

Parking ratio is 1.25 spaces per unit, and 1.01 spaces per bedroom.

Bike parking is provided at a rate of one stall per unit.

Refuse

A refuse room internal to the building will be provided to conceal trash and recycling dumpsters. Access for removal will be provided at Third Street / the East end of the building.

Outdoor Space

There will be a private outdoor balcony provided for each dwelling unit. These balconies are either mounted to the exterior wall, or recessed - refer to the building plans and elevations

The first floor corner commercial space is intended to have outdoor seating, potentially for dining use.

Exterior Design

The goal of the development aesthetic is to compliment the historic Main Street appeal of Mount Horeb. The materials used will be a combination of quality masonry (brick and stone), storefront glazing and composite wood siding. Colors will be selected to complement the streetscape in neutral, timeless, tones.

During our prior concept presentations to the historical commission and the Planning Commission, we heard many comments regarding the building design, and have tried to implement these when feasible:

- Residents prefer a uniqueness to their architecture, one that reinforces the Norwegian culture. There is a corner element at Main Street and Third Street to help provide prominence and anchorage to this street corner.
- We have introduced canopy structures to enhance the pedestrian scale
- Viking/Norwegian/Scandinavian architectural detail elements have been added to the façade, such as window trim, cornice, brickwork, fries banding and gable roofs
- The building is stepped back in areas at the upper floors to reduce the visual scale.
- Parking is concealed, in lieu of exposed surface parking
- A combination of brick, stone, and wood is used to vary the elevation and provide the 'infill' appearance common to most historic villages
- Landscape and decorative lighting will be used to enhance the aesthetic and experience.
- Outdoor spaces have been provided to activate the Main Street presence
- Detail elements common to Mount Horeb are being introduced, such as recessed balconies, large storefront windows, a combination of fabric and structured canopies, brick pilasters, and wood sign-bands. All of these are found in historical context within the Village of Mount Horeb.

The building height is proposed to be three stories, and approximately 41' to the upper parapet, a reduction from prior submittals. Additionally, there are two noteworthy considerations that we feel make this submission worthy of acceptance:

- The building facade is reduced in height, and stepped back in some locations at Main Street such that the entire street-façade is two or three stories in height.
- The topography of the site slopes nearly eight feet from south to north, creating a challenge for any building development. Measured from grade, the exterior walls are approximately 32' tall on the north side, and typically a maximum of only 36 feet in height when measured along any grade at any side.

Roofing is to be low-sloped EPDM membrane over the major roof elements. The gable roof areas (canopy) is intended to be cedar shake appearance. There will be green roof over two canopies and in some roof areas.

The building section in the attached drawings illustrates the relative height relationships to Main Street, the North alley, and refence points to the historic opera house building. It should be noted that this proposed development is now shorter in height relative to the opera house, and approximately the same height as the Village Hall.

Multiple elevations and renderings of the intended design are provided & attached. Materials and colors are shown approximate and called out on the elevations.

Stormwater Management

The redevelopment consideration of this property will not trigger a stormwater management plan. Rainwater collected from hardscape and rooves will be directed to the Village stormwater infrastructure.

There will be placement of rooftop stormwater retention trays to reduce runoff.

Utility Infrastructure Assessment

Initial discussions with Village staff and utility stakeholders have not produced any concern for the development. Adequate supply for all utilities appears to be available, and the future design submissions will accurately describe and design the necessary development infrastructure.

Traffic

A Traffic Impact Analysis (TIA) has been performed by Traffic Analysis and Design (TADI) of Cedarburg, WI. A draft of the TIA report, dated August 14, 2025, has been forward separately to Village Staff. This report indicates only a modest impact to the adjacent streets as caused by this development.

Further, we have modified the indoor parking structure and interior traffic flow such that the 3rd street will be “entrance only” and the NW corner will be “exit only”. This will then send traffic directly to the 2nd street stoplight.

It will be up to the village to determine if the alley should be one way or two way traffic. The developer is ok with either choice.

We are suggesting that the third street parking that exists today adjacent the building, be modified such that it is for short term “Delivery parking” only. This will dedicate spots for retail commercial deliveries and prevent long term parking near the intersection.

Gallina company additional studied the inflow/exit of two of their properties during the exact same peak times the traffic analysis was done – results attached separately.

- Hoff Mt Horeb, WI – 48 units
- LeSilve Middleton, WI – 180 units

Adherence to the Comprehensive Plan

There are several objectives that the 2022 Comprehensive Plan (with amendments) seeks to achieve. The following are examples of how this development

Foster New Housing Options:

- Provide a variety of housing types, densities, arrangements and costs to accommodate the needs, desires and life cycles of existing and future residents.
 - This proposed development will bring over forty new units of apartments within Mount Horeb's urban district.
- Increase the amount, type, and availability of affordable and workforce housing options in the Village.
 - While these units will be considered 'market rate', they provide a housing option for those that may not wish to, or be able to, afford the single-family housing ownership.

Grow the Local Economy:

- Strengthen and diversify the non-residential tax base and employment opportunities to serve the day-to-day needs of residents.
- By adding commercial space and residential housing alike on underperforming real estate, the downtown tax base will be improved, and there will be an improved means to attract and retain businesses that enhance Mount Horeb's desired character and appearance.
 - This development will enhance the downtown character and business climate through new development of properties that have exceeded their lifespan.
- Cultivate independent small businesses and entrepreneurship opportunities in the Village, especially in and around downtown.
 - This development provides both housing and leasable commercial space for small business and is vital to the livelihood of Main Street.

An excerpt from the Comprehensive Plan (Chapter Six: Land Use, page 43) supports the exact type of development our team is proposing:

Central Mixed Use

This future land use category is intended for pedestrian-oriented commercial, office, community facility, and upper story residential uses in a “downtown” setting, with on-street parking and minimal building setbacks. The Central Mixed-Use land use category includes the historic downtown area. The existing Village zoning district that is most appropriate to implement this future land use category is the Central Business District (CB).

- *Policies and Programs:*

- 1. Continue to implement the recommendations of the Downtown Redevelopment Plan.*
- 2. Promote downtown as a tourist, commercial, civic, and social center.*
- 3. Work with downtown property owners and businesses to preserve, renovate, modernize, and put to active use historically significant buildings.*
- 4. Support the expansion, retention, and upgrading of specialty retail, restaurants, financial services, offices, neighborhood retail and services, and community uses.*
- 5. Require that all projects submit and have approved detailed building elevations and site plans, showing the proposed locations of the building(s), parking, storage, loading, signage, landscaping, and lighting prior to development approval.*
- 6. Protect the unique quality of downtown by requiring buildings to be at least two stories with a zero-lot line front setback.*
- 7. Promote truly mixed-use development with residential above the first floor and design oriented to pedestrian and not automobiles.*

The Comprehensive Plan discusses a desire to create housing diversity for empty-nesters, young professionals, and new families, the project’s target demographic. This same plan seeks to “Foster Redevelopment, Reinvestment, and (promote) New Development”.

“Environmental Sustainability and Stewardship” is also a stated goal of the Plan. By creating more density in this pedestrian friendly location, the impact to utility services, sprawling land use, environmental pollutants, and needless consumption of natural habitat is minimized. Simply put, this type of development, with this type of density, is smart.

We believe this well-placed mixed use development not only reinforces the downtown Main Street corridor, it extends the useable area by nearly $\frac{3}{4}$ of a block, Lastly, it aids in reducing urban sprawl through a smart, dense, and walkable land use.

REVISIONS SINCE 9/22/25 SUBMITTAL:

Subsequent to the October 22, 2025 Planning Commission meeting, our team made several significant changes to address many of the stated concerns, including:

1. Building size and height Removed the 4th story — the building is now 3 stories to complement the downtown district. Height will be similar to the municipal building and significantly less than the Opera House roof.
2. Density: Removed 19 units — the building is now proposed to contain 44 dwelling units.
3. Parking: Added parking — now greater than 1 stall per bedroom and 1.25 stalls per unit, for a total of 55 stalls for 44 units (54 bedrooms). Note the Village planner recommended 1.2 stalls per unit.
4. Traffic flow: Proposing one-way traffic. Traffic would only enter from 3rd St. Enclosed parking lot would have one-way traffic flow. Traffic would only exit onto 2nd St. where there is a stoplight.
5. Floor Area Ratio: Reduced Floor Area Ratio from 3.04 to 2.36, well below the 3.0 maximum allowed.
6. Commercial/retail space: Increased commercial/retail space 40% to well over 9,000 sq ft, nearly double the available current structures. Also added entries and replaced shadow boxes with storefront.
7. Bicycle parking: Added bicycle parking to exceed the requirement of 1 space per 3 units.

PREVIOUS REVISIONS, SINCE INITIAL SUBMITTALS:

As a recap of the items that were desired by the plan commission and the public, following are those items and how they were addressed in the submittal:

1. Reduce the mass and soften SE corner and create something “quirky”
 - a. Removed 3 story brick corner column
 - b. Added “Viking architectural style” covered canopy
 - c. Opened porches on both levels of SE corner of the bldg.
 - d. Angled the corner/patios
 - e. Create something green- Added green roof on apt entrance flat canopy roof over the SW public/tenant entrance
 - f. Changed the second infill bldg. to clapboard siding and added Viking arch detail on top of windows.
 - g. Added signage and name to represent “Viking house”-
 - i. Viking houses are primarily known as longhouses (Old Norse: langhús), which were large, communal dwellings for extended families and their livestock, as well as workshops and community gathering spaces. While the term "house" wasn't commonly given names like modern homes, the main building could be referred to as a salr (hall) or, if it was a place of feasting and celebration, a mead-hall.
 - h. Added a Viking Crest at SE corner
 - i. Reduced column heights and Added Dragon heads at west end

- j. Removed the door to parking at main st- now interior access
 - k. Added detail areas and matching block to alley side for interest/view of neighbors
 - l. Changed column heights at SW corner
 - m. Added shadow boxes which will house historic or other relevant photos and artifacts
2. Park the site 1:1 minimum per unit - now it is 1.25:1 with added shared parking.
- a. Removed Alley-loaded direct parking and now park the entire site inside the building
 - b. Split the traffic going to third- proposing a 1 way alley, exit only from the alley parking entrance, directing traffic to Second Street.
 - c. Regarding the double loaded parking stalls being proposed - see relevant project below with identical parking utilization in Middleton WI
 - i. Developer – Tim Carey
 - ii. This project is in the City of Middleton – the City recognizes the tandem stalls in the calculations for parking requirements. Tim stated that they were above the City’s requirements as he felt they were short initially.
 - iii. Property Manager – Leaha at Forward Management
 - iv. Project has 1:1 ratio units / stalls including the tandem stalls. The project has been open for almost 2 years and **they have not had any complaints regarding the tandem stalls or not enough parking.** They assign tandem stalls to their 4th floor units that face the lake (premium units) and this has been received well by those tenants. They have 8 tandem stalls.
 - v. Opened in Fall of 2023
3. Reduced the east side elevation and added detail to Third St façade.
4. Created/kept retail or retail look on the entire frontage on main.
5. Reduce Density. Unit counts have been reduced by 5 units from prior submissions.

Conclusion

Our team has been working tirelessly to accommodate Village feedback while ensuring a viable project. We thank you for your consideration of this exciting development in Mount Horeb, we look forward to continued discussion and collaboration!

Respectfully submitted



Steve Shulfer, AIA
Sketchworks Architecture, LLC

November 26, 2025

Nicholas Owen, Village Administrator
Village of Mount Horeb
138 E Main Street
Mount Horeb, WI 53572

Mr. Owen, and the Mount Horeb Village Board Members:

This letter is written as a reminder of the revisions and concessions made by the developer in recent weeks, to the “Langhus on Main” mixed-use development proposal. We felt it important to list these items separately, and to communicate the most recent measures taken to mitigate any safety concerns and as much as possible the neighbors concerns.

LATEST REVISIONS FROM 10/22 AND 11/19 PLAN COMMISSION MEETINGS:

1. Step back west end of building at MHTC to create more distance, natural lighting, and ease of maintenance.
2. Remove solid wall at Northeast corner to reduce scale and bring in more light directly to the house to the north, and to provide greater visibility
3. Building separation at West is approximately 3-feet from MHTC at SE corner. Added gates and shared keys to prevent vandalism and walk-through
4. Mirrors at garage exit to improve visibility for vehicles exiting garage in alley.
5. Signage at garage exits and entrances to clearly identify traffic flow for both vehicles and pedestrians on the sidewalk.
6. Relocated windows on north elevation to offer more privacy to northern neighbor.

OTHER SUGGESTIONS, BUT WOULD BE VILLAGE ACTIONS TO CONSIDER:

1. Striping in front of drive entrances at pavement at sidewalk and alley for better pedestrian awareness on sidewalk/alley
2. Speed-bumps in alley to slow traffic and create better awareness of garage exit
3. Loading zone or short term parking proposed north of 3rd St. intersection to allow ease of deliveries and to remove any concern of long-term parking in those 2 spots.
4. Remove one parking stall at 2nd St. (proposed, Village choice) at the west alley intersection on the south side to allow more visibility when exiting the alley to 2nd St. stop light.

REVISIONS SINCE 9/22/25 SUBMITTAL:

Subsequent to the October 22, 2025 Planning Commission meeting, our team made several significant changes to address many of the stated concerns, including:

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 - f. Changed the second infill bldg. to clapboard siding and added Viking arch detail on top of windows.
 - g. Added signage and name to represent “Viking house”-
 - i. Viking houses are primarily known as longhouses (Old Norse: langhús), which were large, communal dwellings for extended families and their livestock, as well as workshops and community gathering spaces. While the term "house" wasn't commonly given names like modern homes, the main building could be referred to as a salr (hall) or, if it was a place of feasting and celebration, a mead-hall.
 - h. Added a Viking Crest at SE corner
 - i. Reduced column heights and Added Dragon heads at west end
 - j. Removed the door to parking at Main St.- now interior access
 - k. Added detail areas and matching block to alley side for interest/view of neighbors
 - l. Changed column heights at SW corner
 - m. Added shadow boxes which will house historic or other relevant photos and artifacts

13. Park the site 1:1 minimum per unit - now it is 1.25:1 with added shared parking.
 - n. Removed Alley-loaded direct parking and now park the entire site inside the building
 - o. Split the traffic going to third- proposing a 1-way alley (proposed, Village choice), exit only from the alley parking entrance, directing traffic to Second Street.
 - p. Regarding the double-loaded parking stalls being proposed - see relevant project (examples presented) with identical parking utilization in Middleton WI
14. Reduced the east side elevation and added detail to Third St façade.
15. Created/kept retail or retail look on the entire frontage on main.
16. Reduce Density. Unit counts have been reduced by 5 units from prior submissions. (then reduced again at after 9/22/25)

Conclusion

Community engagement on this project has been incredible, and the developer has gone great lengths to address and mitigate every concern brought forth.

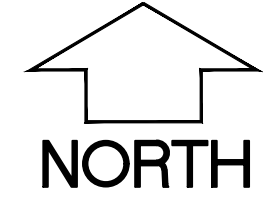
We firmly believe that this mixed-use development should be approved as it strengthens the heart of downtown and Main Street, and positions the community for long-term vitality. So many community's downtowns die for lack of attention to their main street business districts: lack of business, lack of walkability (a condition of sprawl), and lack of residential density downtown. This project resolves all of these risks by bringing daily activity to the village center, supporting local businesses and creating a walkable, engaging environment – a huge economic and benefit, particularly when compared to the current land use (vacant lot, two old converted retail buildings).

In addition to the enhanced and diversified tax base, this project ensures that Main Street remains vibrant and economically resilient for decades to come. It is an investment in Mount Horeb's future character, livability, and sustained success!

Respectfully submitted



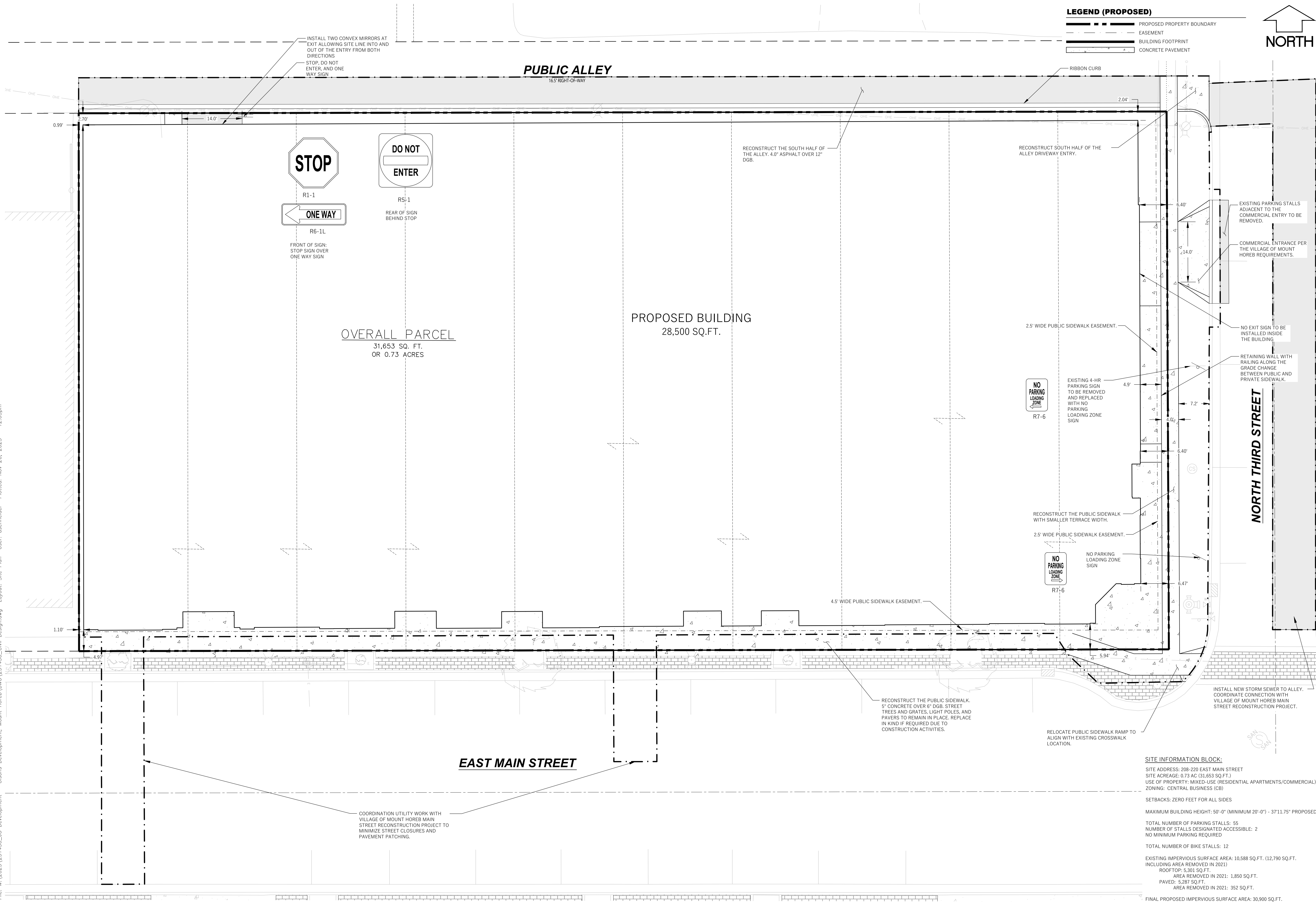
Steve Shulfer, AIA
Sketchworks Architecture, LLC



WYSER
ENGINEERING

LEGEND (PROPOSED)

- PROPOSED PROPERTY BOUNDARY
- EASEMENT
- BUILDING FOOTPRINT
- CONCRETE PAVEMENT



SITE INFORMATION BLOCK:
 SITE ADDRESS: 208-220 EAST MAIN STREET
 SITE ACREAGE: 0.73 AC (31,653 SQ.FT.)
 USE OF PROPERTY: MIXED-USE (RESIDENTIAL APARTMENTS/COMMERCIAL)
 ZONING: CENTRAL BUSINESS (CB)
 SETBACKS: ZERO FEET FOR ALL SIDES
 MAXIMUM BUILDING HEIGHT: 50'-0" (MINIMUM 20'-0") - 37'11.75" PROPOSED
 TOTAL NUMBER OF PARKING STALLS: 55
 NUMBER OF STALLS DESIGNATED ACCESSIBLE: 2
 NO MINIMUM PARKING REQUIRED
 TOTAL NUMBER OF BIKE STALLS: 12
 EXISTING IMPERVIOUS SURFACE AREA: 10,588 SQ.FT. (12,790 SQ.FT. INCLUDING AREA REMOVED IN 2021)
 ROOFTOP: 5,301 SQ.FT.
 AREA REMOVED IN 2021: 1,850 SQ.FT.
 PAVED: 5,287 SQ.FT.
 AREA REMOVED IN 2021: 352 SQ.FT.
 FINAL PROPOSED IMPERVIOUS SURFACE AREA: 30,900 SQ.FT.

LANGHUS ON MAIN
 VILLAGE OF MOUNT HOREB, DANE COUNTY, WI
 Sheet Title:
 210-214 E MAIN STREET
 MOUNT HOREB, WI 53572

Revisions:

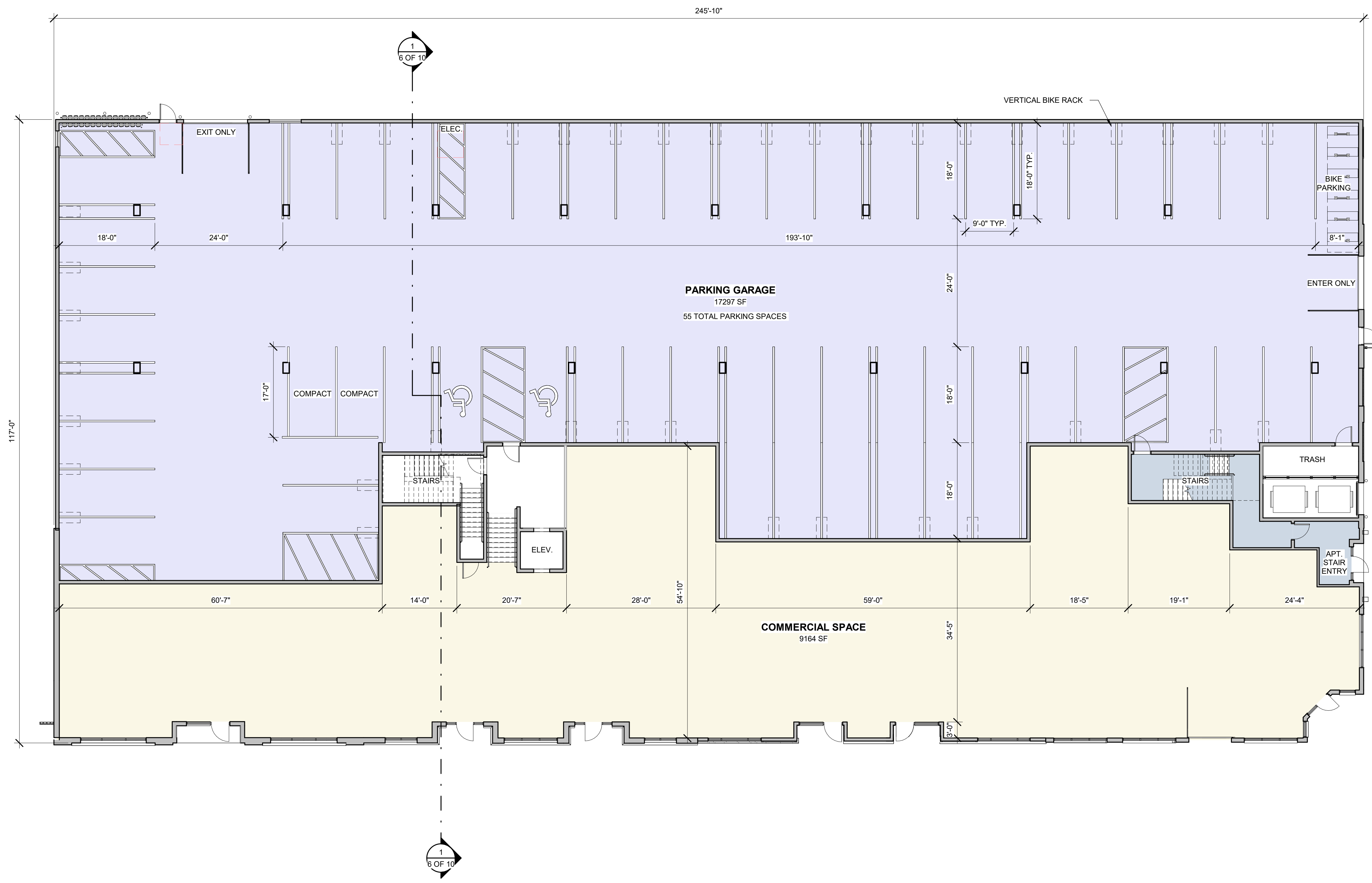
No.	Date:	Description:

Graphic Scale

 WYSER Number: 25-1430
 Set Type: SIP
 Date Issued: 11/26/2025
 Sheet Number: C100

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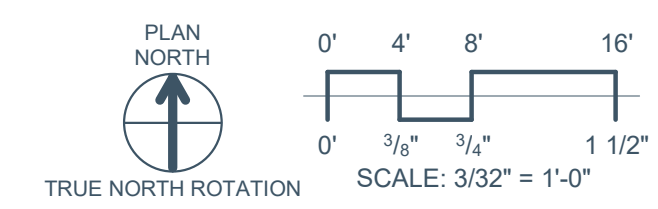
- PARKING STALLS: 55 TOTAL
- BEDROOM COUNT: 54 TOTAL
- RESIDENTIAL UNITS: 44 TOTAL
 - a. (8) STUDIO UNITS
 - b. (20) 1 BED UNITS
 - c. (6) 1 BED / DEN
 - d. (10) 2 BED UNITS
 - e. COMMERCIAL SPACE: 9,164 SF



FIRST FLOOR PLAN - COMMERCIAL SPACE, PARKING

LANGHUS ON MAIN

FLOOR PLANS



DATE: 11/26/25 1 OF 10

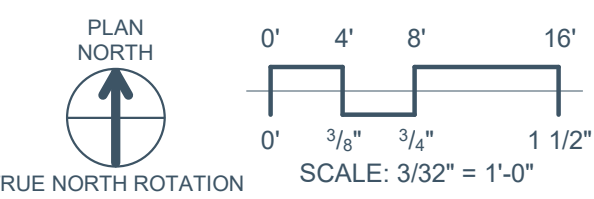
- SECOND FLOOR**
22 TOTAL UNITS
- a. (4) STUDIO
 - GROSS SF: 2,397
 - b. (10) 1 BED
 - GROSS SF: 8,056
 - c. (3) 1 BED / DEN
 - GROSS SF: 2,846
 - d. (5) 2 BED
 - GROSS SF: 5,839



SECOND FLOOR CONCEPT PLAN

LANGHUS ON MAIN

FLOOR PLANS

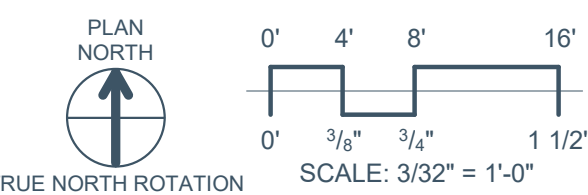


DATE: 11/26/25 2 OF 10

- THIRD FLOOR**
 22 TOTAL UNITS
- a. (4) STUDIO
 - GROSS SF: 2,290
 - b. (10) 1 BED
 - GROSS SF: 7,694
 - c. (3) 1 BED / DEN
 - GROSS SF: 2,579
 - d. (5) 2 BED
 - GROSS SF: 5,533



THIRD FLOOR - CONCEPT PLAN



DATE: 11/26/25 3 OF 10

LANGHUS ON MAIN

FLOOR PLANS

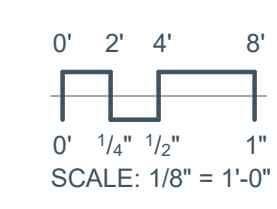
EXTERIOR ELEVATIONS KEYNOTE SCHEDULE		
#	DESCRIPTION	COLOR
1	CLAY BRICK MASONRY - FULL BED	HISTORIC RED BRICK
2	CLAY BRICK MASONRY - FULL BED	TAN / CREAM
3	LIMESTONE MASONRY	CREAM / LIGHT BLEND
4	LP SMARTSIDE SIDING	CREAM COLOR
5	LP SMARTSIDE SIDING	BROWN / GREY COLOR
6	LP SMARTSIDE SIDING	DARK BRICK COLOR - MATCH
7	CONCRETE MASONRY UNIT	LIGHT BLEND
8	CONCRETE MASONRY UNIT	DARK BRICK COLOR - MATCH
9	CEDAR SHAKE ROOF	MEDIUM - LIGHT WOOD
10	METAL CANOPY	BRONZE TONE
11	CORBLE ACCENT PANEL	MEDIUM - DARK WOOD
12	ACCENT PANEL	MEDIUM - DARK WOOD
13	METAL BALCONY AND RAILING	MEDIUM - DARK BRONZE
14	CONCRETE MASONRY COPING/ WALL CAP	LIGHT GREY/ CONCRETE
15	CORBLE ACCENT PANEL	LIGHT COLOR ACCENT
16	FABRIC AWNING	COLOR TBD
17	COMPOSITE WOOD TRIM BOARDS, DECORATIVE	MEDIUM - LIGHT WOOD
18	COMPOSITE WOOD BRACKETS, DECORATIVE	MEDIUM - LIGHT WOOD
19	LINEAR WOOD PANEL SIDING	LIGHT PAINT FINISH
20	LINTEL OVER WINDOWS AND OPENINGS	MEDIUM - LIGHT MASONRY UNIT OR FAUX WOOD ELEMENT
21	CLAY BRICK MASONRY - FULL BED	BROWN BLEND
22	GREEN ROOF	-



WEST ELEVATION - COLORED



SOUTH ELEVATION - COLORED



LANGHUS ON MAIN

ELEVATIONS

DATE: 11/26/25 4 OF 10

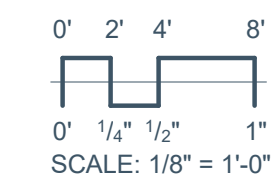
EXTERIOR ELEVATIONS KEYNOTE SCHEDULE		
#	DESCRIPTION	COLOR
1	CLAY BRICK MASONRY - FULL BED	HISTORIC RED BRICK
2	CLAY BRICK MASONRY - FULL BED	TAN / CREAM
3	LIMESTONE MASONRY	CREAM / LIGHT BLEND
4	LP SMARTSIDE SIDING	CREAM COLOR
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6	LP SMARTSIDE SIDING	DARK BRICK COLOR - MATCH
7	CONCRETE MASONRY UNIT	LIGHT BLEND
8	CONCRETE MASONRY UNIT	DARK BRICK COLOR - MATCH
9	CEDAR SHAKE ROOF	MEDIUM - LIGHT WOOD
10	METAL CANOPY	BRONZE TONE
11	CORBLE ACCENT PANEL	MEDIUM - DARK WOOD
12	ACCENT PANEL	MEDIUM - DARK WOOD
13	METAL BALCONY AND RAILING	MEDIUM - DARK BRONZE
14	CONCRETE MASONRY COPING/ WALL CAP	LIGHT GREY/ CONCRETE
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16	FABRIC AWNING	COLOR TBD
17	COMPOSITE WOOD TRIM BOARDS, DECORATIVE	MEDIUM - LIGHT WOOD
18	COMPOSITE WOOD BRACKETS, DECORATIVE	MEDIUM - LIGHT WOOD
19	LINEAR WOOD PANEL SIDING	LIGHT PAINT FINISH
20	LINTEL OVER WINDOWS AND OPENINGS	MEDIUM - LIGHT MASONRY UNIT OR FAUX WOOD ELEMENT
21	CLAY BRICK MASONRY - FULL BED	BROWN BLEND
22	GREEN ROOF	-



EAST ELEVATION - COLORED



NORTH ELEVATION - COLORED



LANGHUS ON MAIN

ELEVATIONS

DATE: 11/26/25 5 OF 10



3D VIEW - SOUTH EAST



3D VIEW - SOUTH WEST



3D VIEW - NORTH EAST

LANGHUS ON MAIN

STREET VIEWS

DATE: 11/14/25 7 OF 10

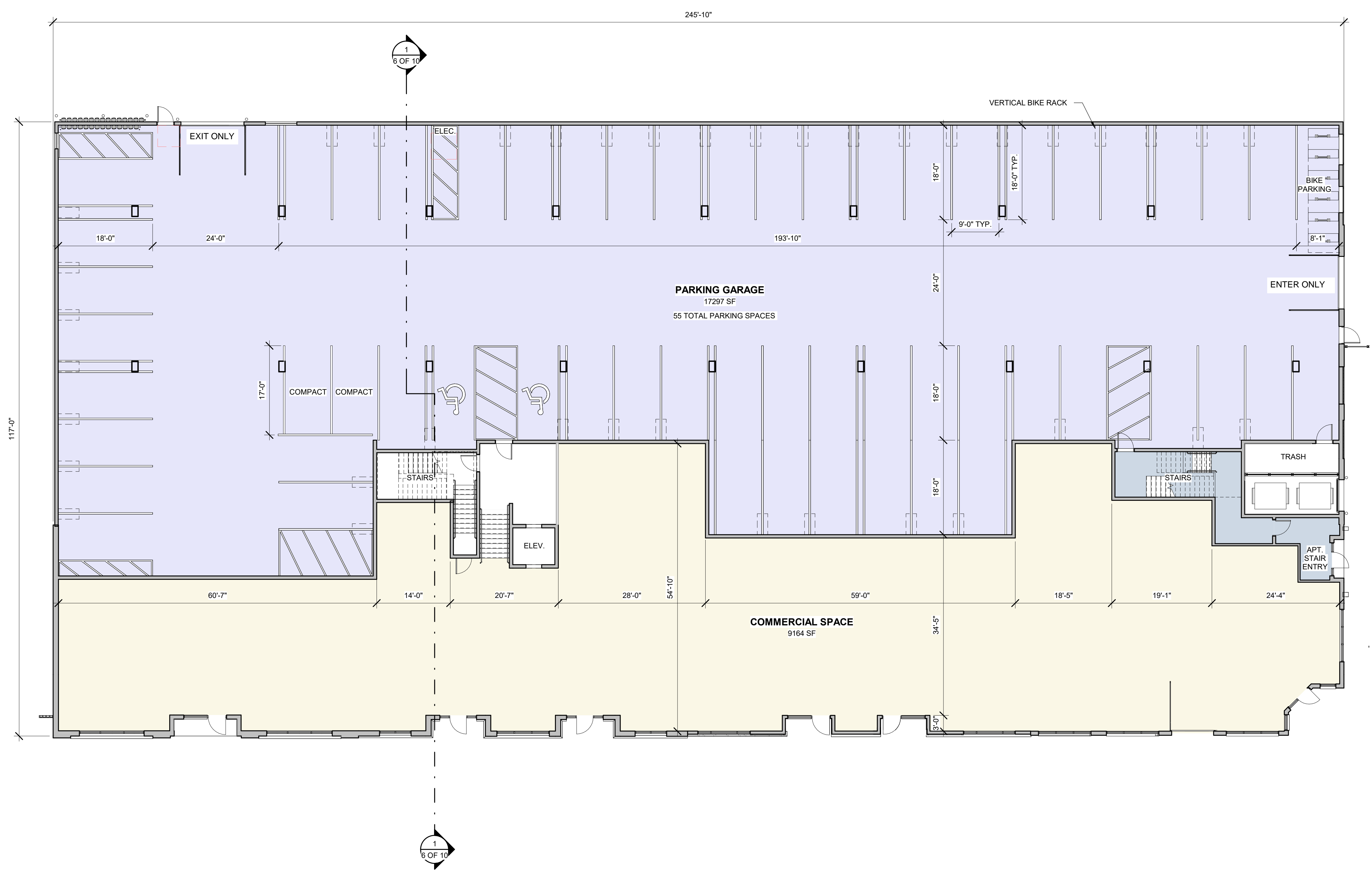


LANGHUS ON MAIN

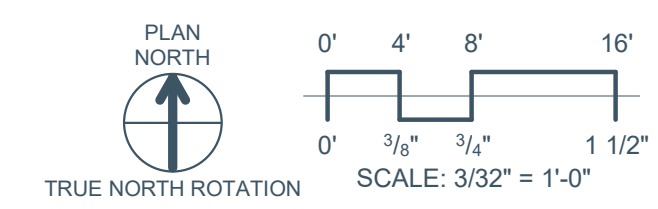
GDP / SIP SUBMITTAL

DATE: 11/14/2025 COVER

- PARKING STALLS: 55 TOTAL
- BEDROOM COUNT: 54 TOTAL
- RESIDENTIAL UNITS: 44 TOTAL
 - a. (8) STUDIO UNITS
 - b. (20) 1 BED UNITS
 - c. (6) 1 BED / DEN
 - d. (10) 2 BED UNITS
 - e. COMMERCIAL SPACE: 9,164 SF



FIRST FLOOR PLAN - COMMERCIAL SPACE, PARKING



DATE: 11/14/25 2 OF 13

LANGHUS ON MAIN

FLOOR PLANS

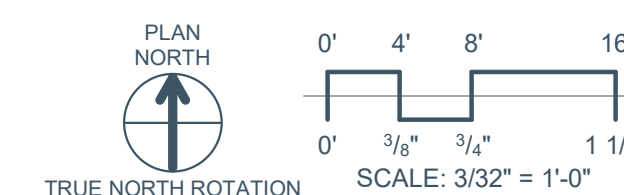
- SECOND FLOOR**
22 TOTAL UNITS
- a. (4) STUDIO
 - GROSS SF: 2,338
 - b. (10) 1 BED
 - GROSS SF: 7,900
 - c. (3) 1 BED / DEN
 - GROSS SF: 2,786
 - d. (5) 2 BED
 - GROSS SF: 5,891



SECOND FLOOR CONCEPT PLAN

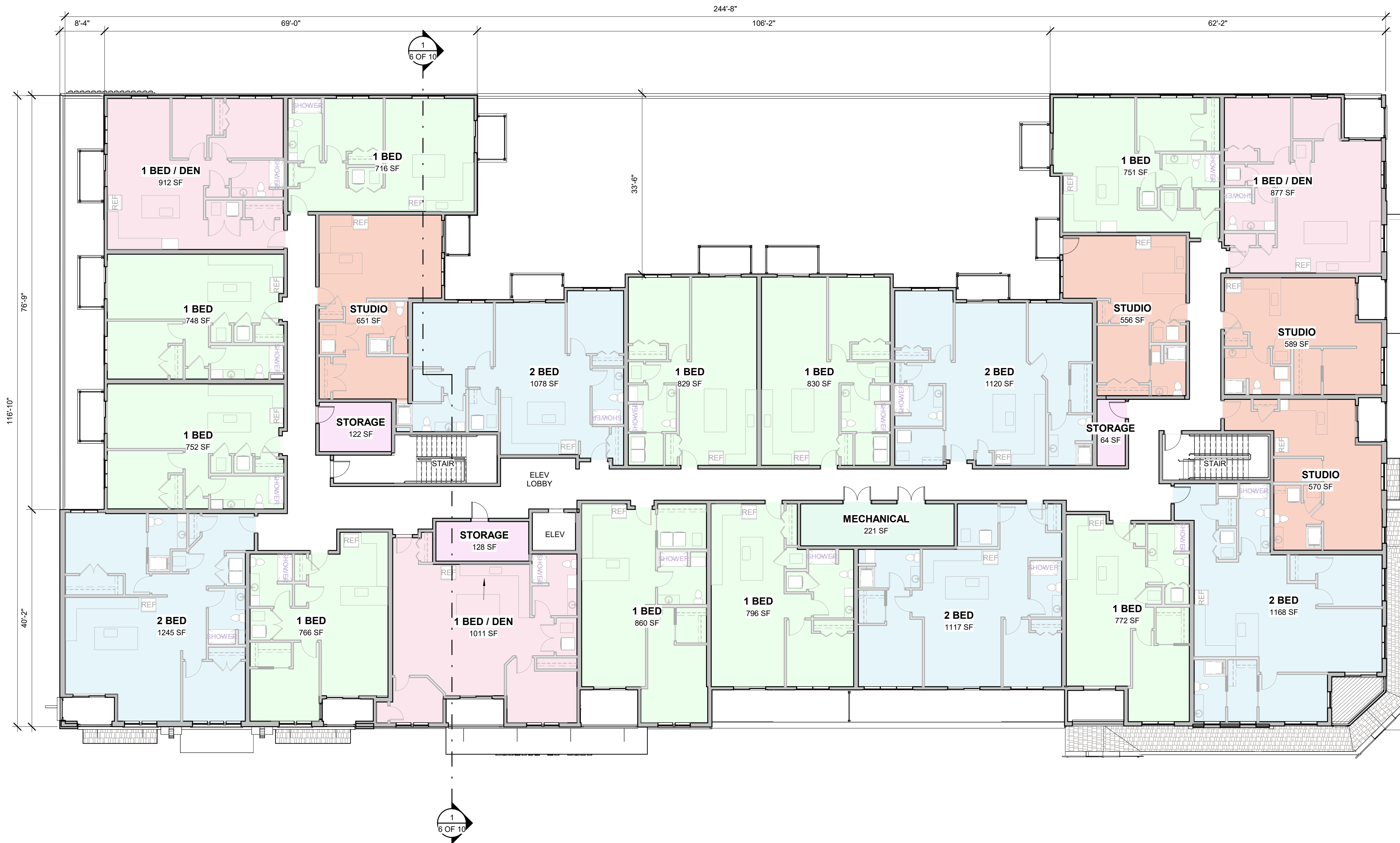
LANGHUS ON MAIN

FLOOR PLANS

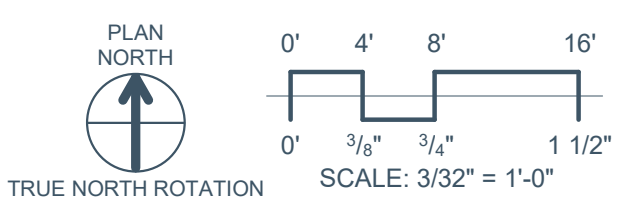


DATE: 11/14/25 3 OF 13

- THIRD FLOOR**
 22 TOTAL UNITS
- a. (4) STUDIO
 - GROSS SF: 2,295
 - b. (10) 1 BED
 - GROSS SF: 7,806
 - c. (3) 1 BED / DEN
 - GROSS SF: 2,828
 - d. (5) 2 BED
 - GROSS SF: 5,805



THIRD FLOOR - CONCEPT PLAN



DATE: 11/14/25 4 OF 13

LANGHUS ON MAIN

FLOOR PLANS

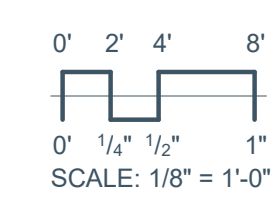
EXTERIOR ELEVATIONS KEYNOTE SCHEDULE		
#	DESCRIPTION	COLOR
1	CLAY BRICK MASONRY - FULL BED	HISTORIC RED BRICK
2	CLAY BRICK MASONRY - FULL BED	TAN / CREAM
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8	CONCRETE MASONRY UNIT	DARK BRICK COLOR - MATCH
9	CEDAR SHAKE ROOF	MEDIUM - LIGHT WOOD
10	METAL CANOPY	BRONZE TONE
11	CORBLE ACCENT PANEL	MEDIUM - DARK WOOD
12	ACCENT PANEL	MEDIUM - DARK WOOD
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15	CORBLE ACCENT PANEL	LIGHT COLOR ACCENT
16	FABRIC AWNING	COLOR TBD
17	COMPOSITE WOOD TRIM BOARDS, DECORATIVE	MEDIUM - LIGHT WOOD
18	COMPOSITE WOOD BRACKETS, DECORATIVE	MEDIUM - LIGHT WOOD
19	LINEAR WOOD PANEL SIDING	LIGHT PAINT FINISH
20	LINTEL OVER WINDOWS AND OPENINGS	MEDIUM - LIGHT MASONRY UNIT OR FAUX WOOD ELEMENT
21	CLAY BRICK MASONRY - FULL BED	BROWN BLEND
22	GREEN ROOF	-



WEST ELEVATION - COLORED



SOUTH ELEVATION - COLORED



LANGHUS ON MAIN

ELEVATIONS

DATE: 11/14/25 5 OF 13

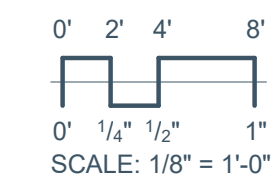
EXTERIOR ELEVATIONS KEYNOTE SCHEDULE		
#	DESCRIPTION	COLOR
1	CLAY BRICK MASONRY - FULL BED	HISTORIC RED BRICK
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22	GREEN ROOF	-



EAST ELEVATION - COLORED



NORTH ELEVATION - COLORED



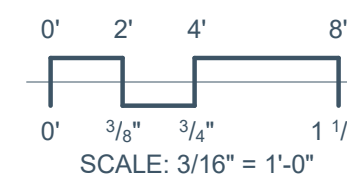
LANGHUS ON MAIN

ELEVATIONS

DATE: 11/14/25 6 OF 13



BUILDING SECTION - GRADE CHANGES



DATE: 11/14/25 7 OF 13

LANGHUS ON MAIN

BUILDING SECTION



3D VIEW - SOUTH EAST



3D VIEW - SOUTH WEST



3D VIEW - NORTH EAST

LANGHUS ON MAIN

STREET VIEWS

DATE: 11/14/25 8 OF 13



LANGHUS ON MAIN

STREET VIEWS

DATE: 11/14/25 9 OF 13



LANGHUS ON MAIN

STREET VIEWS

DATE: 11/14/25 10 OF 13



LANGHUS ON MAIN

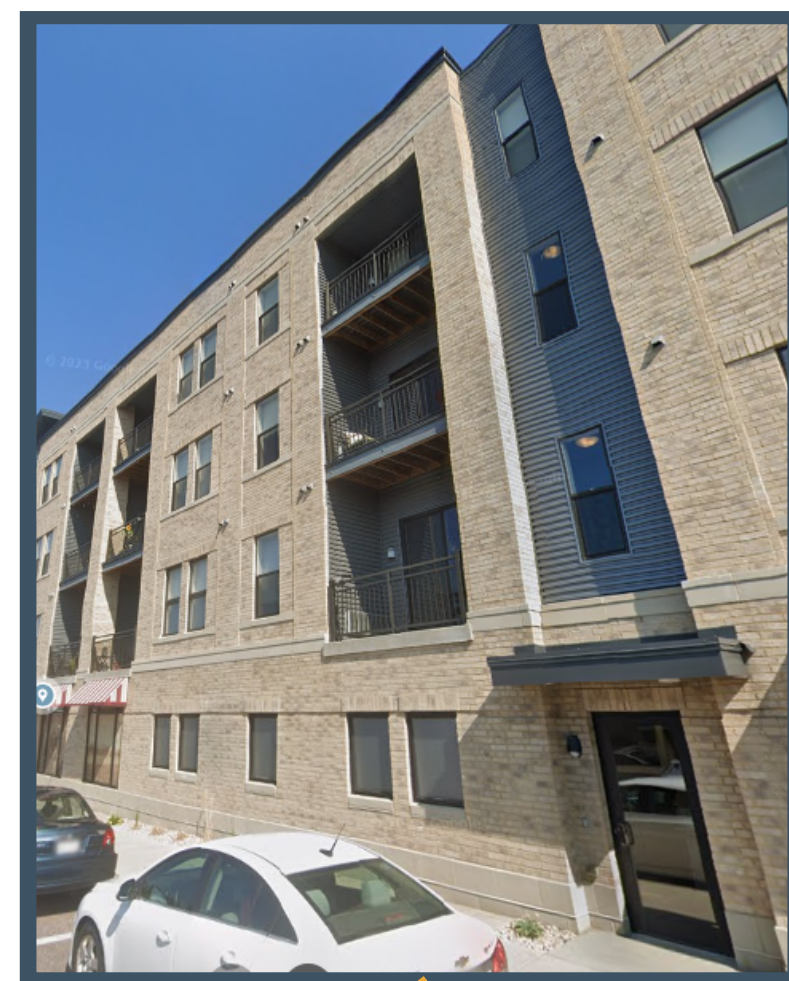
RENDERED STREET IMAGE

DATE: 11/14/25 11 OF 13



LANGHUS ON MAIN

RENDERED STREET IMAGE

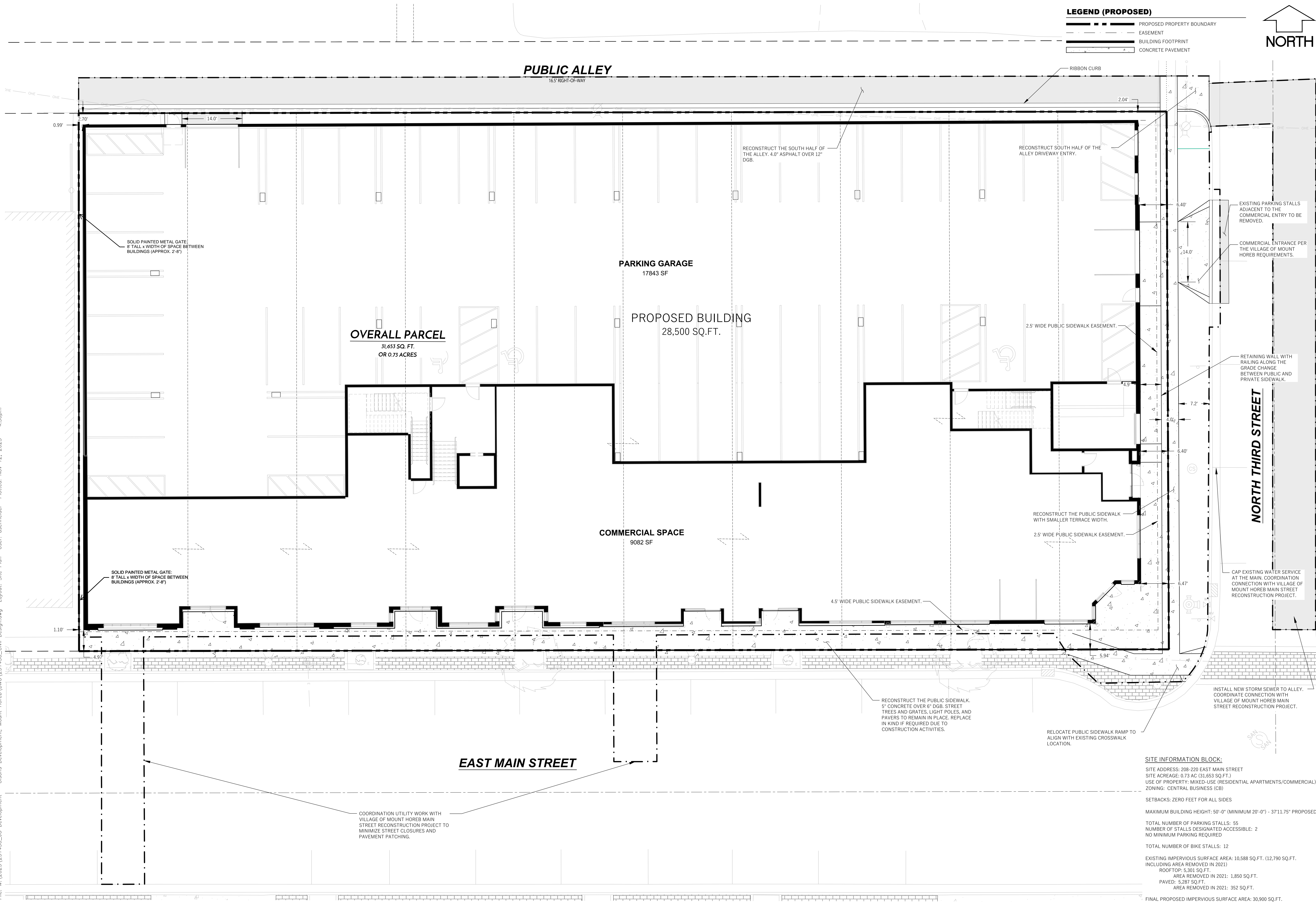


SOUTH ELEVATION - COLOR

LANGHUS ON MAIN

DESIGN INSPIRATION

LEGEND (PROPOSED)
 PROPOSED PROPERTY BOUNDARY
 EASEMENT
 BUILDING FOOTPRINT
 CONCRETE PAVEMENT



OVERALL PARCEL
 31,653 SQ. FT.
 OR 0.73 ACRES

PARKING GARAGE
 17843 SF

PROPOSED BUILDING
 28,500 SQ.FT.

COMMERCIAL SPACE
 9082 SF

NORTH THIRD STREET

EAST MAIN STREET

LANGHUS ON MAIN
 VILLAGE OF MOUNT HOREB, DANE COUNTY, WI

Revisions:

No.	Date:	Description:

Graphic Scale	0' 5' 10' 15'
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Wyser Number	25-1430
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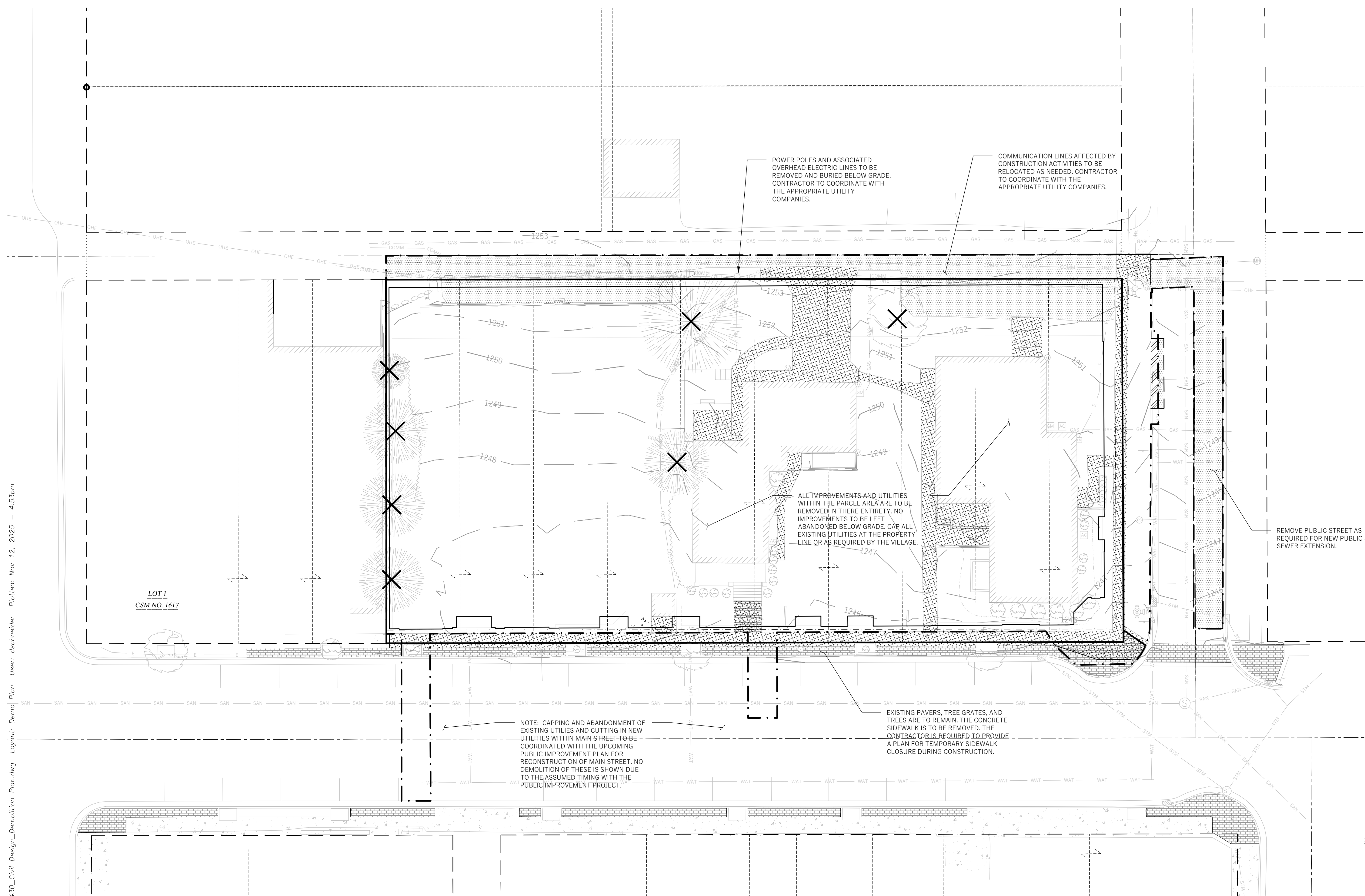
Set Type	SIP
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Date Issued	11/14/2025
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Sheet Number	C100
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SITE INFORMATION BLOCK:
 SITE ADDRESS: 208-220 EAST MAIN STREET
 SITE ACREAGE: 0.73 AC (31,653 SQ.FT.)
 USE OF PROPERTY: MIXED-USE (RESIDENTIAL APARTMENTS/COMMERCIAL)
 ZONING: CENTRAL BUSINESS (CB)
 SETBACKS: ZERO FEET FOR ALL SIDES
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 TOTAL NUMBER OF PARKING STALLS: 55
 NUMBER OF STALLS DESIGNATED ACCESSIBLE: 2
 NO MINIMUM PARKING REQUIRED
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 ROOFTOP: 5,301 SQ.FT.
 AREA REMOVED IN 2021: 1,850 SQ.FT.
 PAVED: 5,287 SQ.FT.
 AREA REMOVED IN 2021: 352 SQ.FT.
 FINAL PROPOSED IMPERVIOUS SURFACE AREA: 30,900 SQ.FT.

File: W:\2025\251430_JG_Development - Olsens Development, Mount Horeb\DWG\251430_Civil_Design\Demolition_Plan.dwg Layout: Demolition_Plan User: dschneider Plotted: Nov 12, 2025 - 4:53pm



LEGEND (PROPOSED)

- SUBJECT PARCEL PROPERTY LINE
- - - CONSTRUCTION LIMIT LINE
- ▭ NEW BUILDING (FOR REFERENCE)
- ▭ SAWCUT LIMITS
- ▨ FULL SECTION ASPHALT REMOVAL AREA
- ▨ CONCRETE REMOVAL AREA
- ▨ CURB AND GUTTER REMOVAL
- ✕ TREE REMOVAL



- GENERAL NOTES**
- UNDERLYING SITE CONTOURS AND INFORMATION BASED ON TOPOGRAPHIC & UTILITY DATA AS SURVEYED BY WYSER ENGINEERING ON THE WEEK OF MAY 27, 2025. WYSER ENGINEERING SHALL NOT BE HELD RESPONSIBLE FOR ANY ERRORS OR OMISSIONS THAT MAY ARISE AS A RESULT OF ERRONEOUS OR INCOMPLETE INFORMATION PROVIDED BY OTHERS. CONTRACTOR TO CONFIRM ALL ELEVATIONS, GENERAL DRAINAGE AND EARTHWORK REQUIREMENTS PRIOR TO CONSTRUCTION.
 - THE BENCHMARK LOCATIONS ARE SHOWN FOR REFERENCE ONLY ON THIS PLAN. THE BENCHMARKS SHALL BE VALIDATED BY LICENSED LAND SURVEYOR PRIOR TO CONSTRUCTION. CONTRACTOR ASSUMES RISK ASSOCIATED WITH BENCHMARK ELEVATIONS UNTIL CONFIRMED.
 - CONTRACTOR TO OBTAIN APPROPRIATE PERMITS FOR STREET OPENINGS & TO WORK WITHIN THE VILLAGE'S LAND IF REQUIRED.
 - WYSER ENGINEERING SHALL BE HELD HARMLESS AND DOES NOT WARRANT ANY DEVIATIONS BY THE OWNER OR CONTRACTOR FROM THE APPROVED CONSTRUCTION PLANS THAT MAY RESULT IN DISCIPLINARY ACTIONS BY REGULATORY AGENCIES.
 - IF ANY ERRORS, DISCREPANCIES, OR OMISSIONS WITHIN THE PLAN BECOME APPARENT, IT SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER PRIOR TO CONSTRUCTION SO THAT CLARIFICATION OR REDESIGN MAY OCCUR.
 - ALL MUNICIPAL UTILITY CONNECTIONS, WORK IN ROW, PUBLIC OUTLOTS AND PUBLIC EASEMENTS SHALL BE IN ACCORDANCE WITH VILLAGE OF MOUNT HOREB STANDARD SPECIFICATIONS.

- DEMOLITION NOTES**
- THIS PLAN INDICATES ITEMS ON THE SITE, NOT INCLUDING INTERNAL BUILDING DEMOLITION, INTENDED FOR DEMOLITION BASED ON THE CURRENT SITE DESIGN THAT HAVE BEEN IDENTIFIED BY A REASONABLE OBSERVATION OF THE EXISTING CONDITIONS THROUGH FIELD SURVEY RECONNAISSANCE (BY OTHERS), "DIGGER'S HOTLINE" LOCATION, AND GENERAL "STANDARD OF CARE". THERE MAY BE ADDITIONAL ITEMS THAT CAN NOT BE IDENTIFIED BY A REASONABLE ABOVE GROUND OBSERVATION, WHERE NOT INCLUDED WITHIN THE FIELD SURVEY, OF WHICH THE ENGINEER WOULD HAVE NO KNOWLEDGE OR MAY BE A PART OF ANOTHER DESIGN DISCIPLINE. IT IS THE CONTRACTOR'S / BIDDER'S RESPONSIBILITY TO REVIEW THE PLANS, INSPECT THE SITE AND PROVIDE HIS OWN DUE DILIGENCE TO INCLUDE IN HIS BID WHAT ADDITIONAL ITEMS, IN HIS OPINION, MAY BE NECESSARY FOR DEMOLITION. ANY ADDITIONAL ITEMS IDENTIFIED BY THE CONTRACTOR / BIDDER SHALL BE IDENTIFIED IN THE BID AND REPORTED TO THE OWNER AND ENGINEER OF RECORD. WYSER ENGINEERING TAKES NO RESPONSIBILITY FOR ITEMS ON THE PROPERTY THAT COULD NOT BE LOCATED BY A REASONABLE OBSERVATION OF THE PROPERTY OR OF WHICH THEY WOULD HAVE NO KNOWLEDGE.
 - PRIOR TO CONSTRUCTION, THE CONTRACTOR IS RESPONSIBLE FOR:
 - EXAMINING ALL SITE CONDITIONS RELATIVE TO THE CONDITIONS INDICATED ON THE ENGINEERING DRAWINGS. ANY DISCREPANCIES ARE TO BE REPORTED TO THE OWNER AND ENGINEER AND RESOLVED PRIOR TO THE START OF CONSTRUCTION.
 - VERIFYING UTILITY ELEVATIONS AND NOTIFYING OWNER AND ENGINEER OF ANY DISCREPANCIES. NO WORK SHALL BE PERFORMED UNTIL THE DISCREPANCIES ARE RESOLVED.
 - NOTIFYING ALL UTILITIES PRIOR TO THE REMOVAL OF ANY UNDERGROUND UTILITIES.
 - NOTIFYING THE OWNER, DESIGN ENGINEER AND LOCAL CONTROLLING MUNICIPALITY 48 HOURS PRIOR TO THE START OF CONSTRUCTION TO ARRANGE FOR APPROPRIATE CONSTRUCTION INSPECTION.
 - CONTRACTOR IS SOLELY RESPONSIBLE FOR SITE SAFETY DURING THE CONSTRUCTION OF THESE IMPROVEMENTS.
 - CONTRACTOR SHALL KEEP ALL STREETS AND ADJOINING SHARED ACCESS ROADWAYS FREE AND CLEAR OF ALL CONSTRUCTION RELATED DIRT, DUST AND DEBRIS.
 - ALL TREES WITHIN THE CONSTRUCTION LIMITS SHALL BE REMOVED UNLESS SPECIFICALLY CALLED OUT FOR PROTECTION. ALL TREES TO BE REMOVED SHALL BE REMOVED IN THEIR ENTIRETY. STUMPS SHALL BE GROUND TO PROPOSED SUBGRADE.
 - PERFORM TREE PRUNING IN ALL LOCATIONS WHERE PROPOSED PAVEMENT AND / OR UTILITY INSTALLATION ENCRUCH WITHIN THE EXISTING DRIP LINE OF THE TREES TO REMAIN. ALL TRENCHING WITHIN THE EXISTING DRIP LINE OF THE TREES TO REMAIN SHALL BE DONE RADIIALLY AWAY FROM THE TRUNK IF ROOTS IN EXCESS OF 1" DIAMETER ARE EXPOSED. ROOTS MUST BE CUT BY REPUTABLE TREE PRUNING SERVICE PRIOR TO ANY TRANSVERSE TRENCHING.
 - ALL LIGHT POLES TO BE REMOVED SHALL BE REMOVED IN THEIR ENTIRETY, INCLUDING BASE AND ALL APPURTENANCES. SALVAGE OR RELOCATION, COORDINATE RELOCATION AND / OR ABANDONMENT OF ALL ELECTRIC LINES WITH ELECTRICAL ENGINEER AND OWNER PRIOR TO DEMOLITION.
 - CONTRACTOR SHALL COORDINATE PRIVATE UTILITY REMOVAL / ABANDONMENT AND NECESSARY RELOCATIONS WITH RESPECTIVE UTILITY COMPANY. COORDINATION REQUIRED PRIOR TO CONSTRUCTION.
 - ABANDONED / REMOVED ITEMS SHALL BE DISPOSED OF OFF SITE UNLESS OTHERWISE NOTED.
 - THE CONTRACTOR SHALL INSTALL A PEDESTRIAN FENCE AROUND ALL EXCAVATIONS TO BE LEFT OPEN OVERNIGHT AS REQUIRED.
 - CONTRACTOR TO REMOVE EXISTING UTILITY PIPE AND BACKFILL WITH SELECT FILL OR PROVIDE PIPE BACK-FILLING WITHIN BUILDING FOOTPRINT USING "LOW DENSITY CONCRETE / FLOWABLE FILL".
 - GRANULAR BACKFILL MATERIALS ARE REQUIRED FOR FILL UNDER PROPOSED PAVED AREAS.
 - RESTORATION OF THE EXISTING RIGHT-OF-WAYS AS NEEDED ARE CONSIDERED INCIDENTAL AND SHOULD BE PART OF THE COST OF THE UNDERGROUND IMPROVEMENTS, DEMOLITION AND REMOVAL. THIS INCLUDES, BUT IS NOT LIMITED TO, CURB & GUTTER, SIDEWALK, TOPSOIL, SEEDING AND MULCHING.
 - ANY SANITARY SEWER, SANITARY SEWER SERVICES, WATER MAIN, WATER SERVICES, STORM SEWER, OR OTHER UTILITIES, WHICH ARE DAMAGED BY THE CONTRACTORS, SHALL BE REPAIRED TO THE OWNER'S SATISFACTION AT THE CONTRACTOR'S EXPENSE.

LANGHUS ON MAIN
 VILLAGE OF MOUNT HOREB, DANE COUNTY, WI
 Sheet Title: DEMOLITION PLAN

210-214 E MAIN STREET
 MOUNT HOREB, WI 53572

Revisions:		
No.	Date:	Description:

Graphic Scale: 0' 5' 10' 20' 30'

Wysér Number: 25-1430

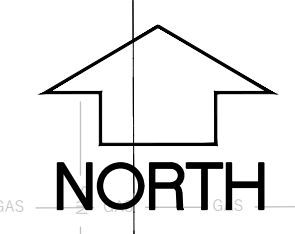
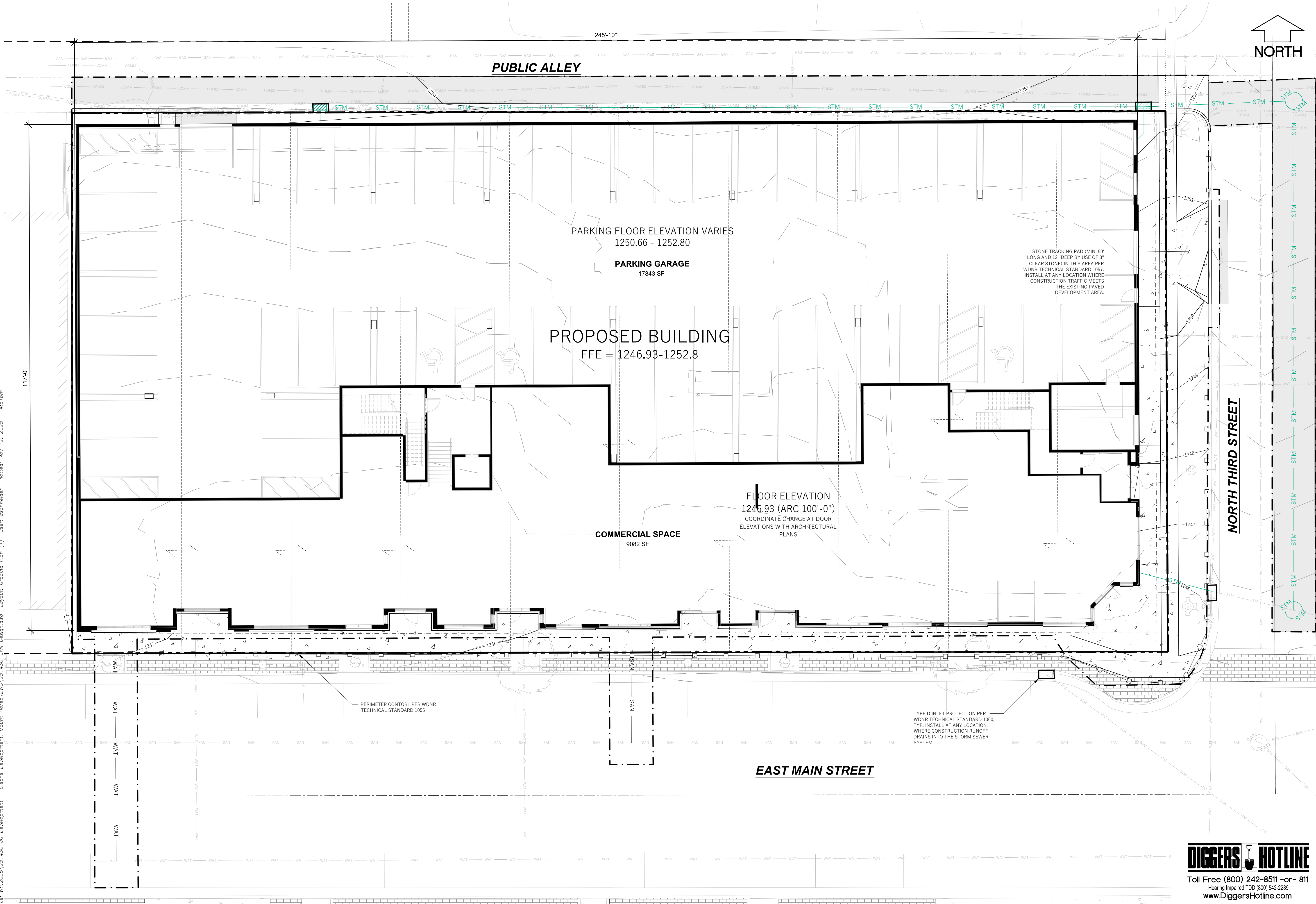
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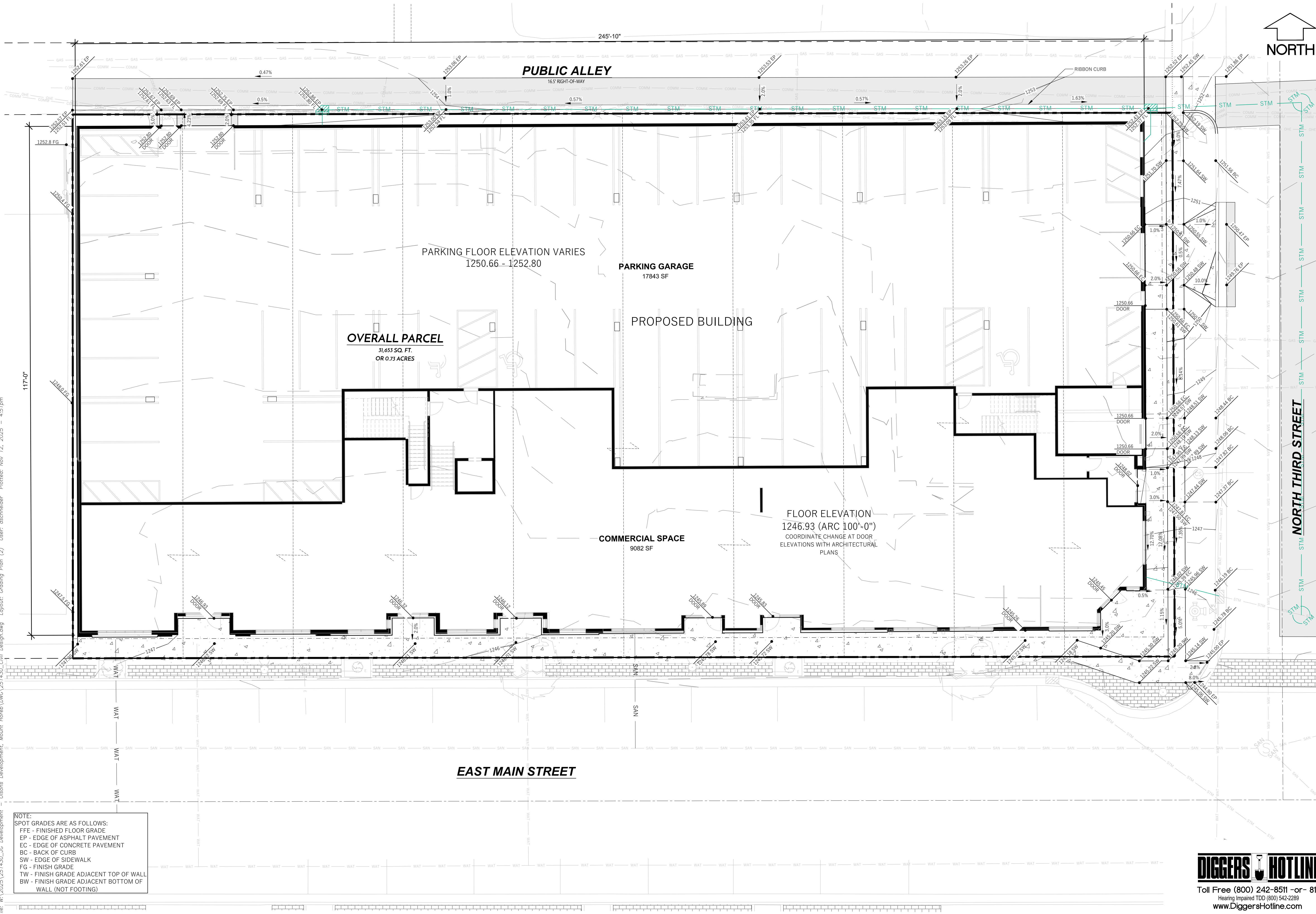
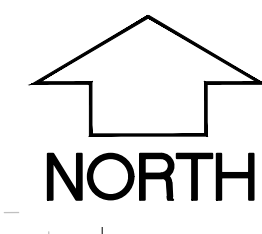
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Revisions:		
No.	Date:	Description:

Graphic Scale	0' 5' 10' 15'
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Set Type	SIP
Date Issued	11/14/2025
Sheet Number	C200

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NOTE:
 SPOT GRADES ARE AS FOLLOWS:
 FFE - FINISHED FLOOR GRADE
 EP - EDGE OF ASPHALT PAVEMENT
 EC - EDGE OF CONCRETE PAVEMENT
 BC - BACK OF CURB
 SW - EDGE OF SIDEWALK
 FG - FINISH GRADE
 TW - FINISH GRADE ADJACENT TOP OF WALL
 BW - FINISH GRADE ADJACENT BOTTOM OF WALL (NOT FOOTING)

LANGHUS ON MAIN
 VILLAGE OF MOUNT HOREB, DANE COUNTY, WI
 Sheet Title:
 DETAILED GRADING PLAN
 210-214 E MAIN STREET
 MOUNT HOREB, WI 53572

No.	Date	Description

Graphic Scale	0' 5' 10' 15'
Wyer Number	25-1430
Set Type	SIP
Date Issued	11/14/2025
Sheet Number	C201

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No.	Date:	Description:

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Wysier Number 25-1430

Set Type SIP

Date Issued 11/14/2025

Sheet Number C300

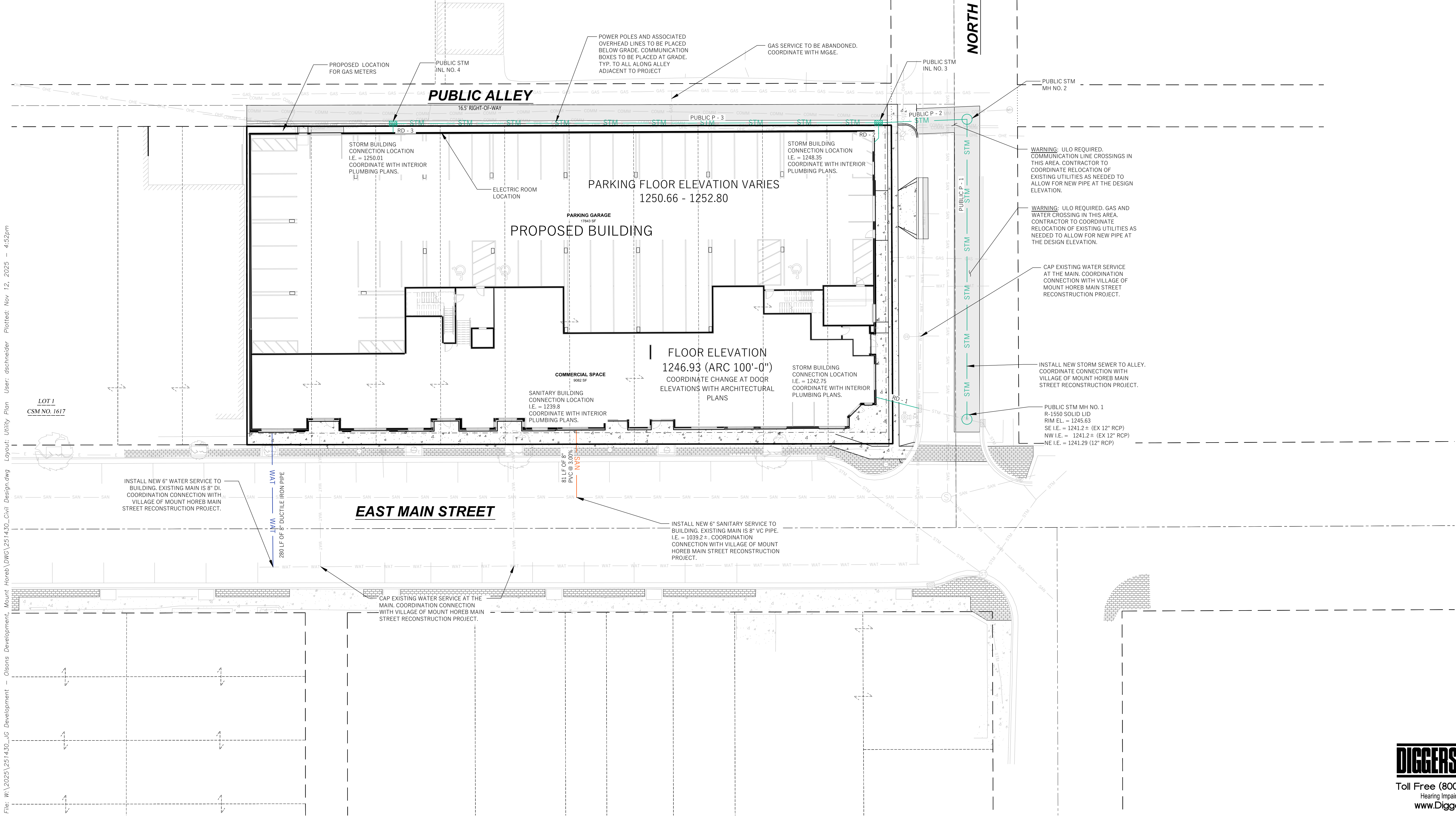
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PIPE LABEL	FROM	TO	LENGTH (FT)	INVERT ELEV. (FT)	DISCHARGE ELEV. (FT)	SLOPE (%)	PIPE SIZE & TYPE	MANNING'S NUMBER	DESIGN RUNOFF (GPM)	PIPE CAPACITY (GPM)
RD - 1	SE ROOF DRAIN	EX INL	17	1242.75	1242.41	2.00%	10" HDPE	0.012	987.75	1506.61
RD - 2	NE ROOF DRAIN	STM INL NO. 3	8	1248.35	1248.18	2.00%	8" HDPE	0.012	238.86	830.95
RD - 3	NW ROOF DRAIN	STM INL NO. 4	4	1250.01	1249.93	2.00%	10" HDPE	0.012	914.14	1506.61

PIPE LABEL	FROM	TO	LENGTH (FT)	INVERT ELEV. (FT)	DISCHARGE ELEV. (FT)	SLOPE (%)	PIPE SIZE & TYPE	MANNING'S NUMBER	DESIGN RUNOFF (GPM)	PIPE CAPACITY (GPM)
P - 1	MH NO. 2	MH NO. 1	118	1247.18	1241.29	5.00%	12" RCP	0.013	2011.14	3575.69
P - 2	STM INL NO. 3	MH NO. 2	33	1247.85	1247.18	2.00%	12" RCP	0.013	2011.14	2261.47
P - 3	STM INL NO. 4	STM INL NO. 3	192	1249.77	1247.85	1.00%	12" RCP	0.013	1396.19	1599.10

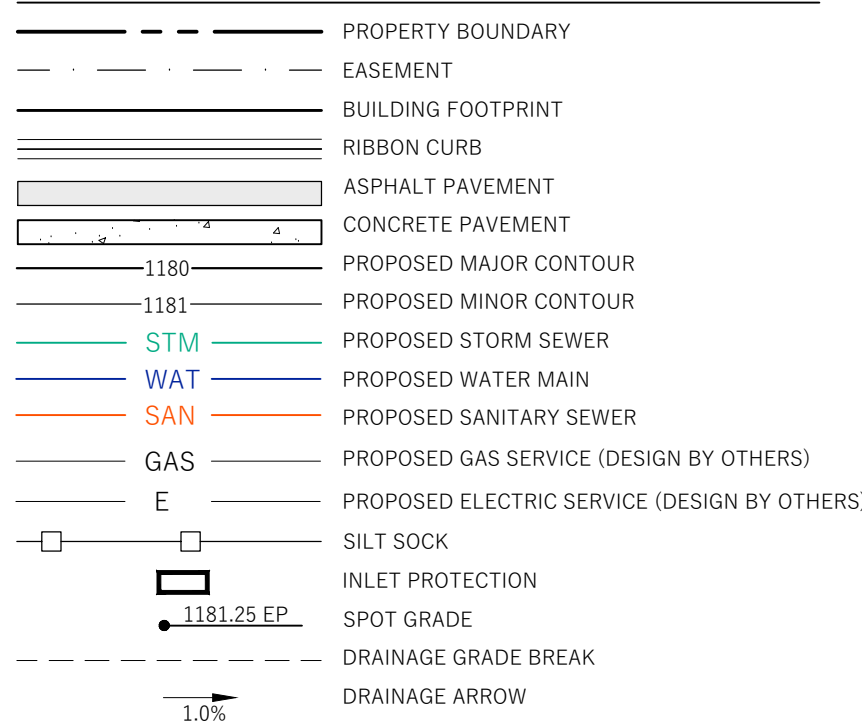
LABEL	INVERT ELEV. (FT)	RIM ELEV. (FT)	DEPTH (FT)	STRUCTURE DESCRIPTION	GRATE
MH NO. 1	1241.19	1245.63	4.44	48" MANHOLE	R-1550, SOLID LID
MH NO. 2	1247.18	1251.75	4.57	48" MANHOLE	R-1550, SOLID LID
STM INL NO. 3	1247.85	1252.43	4.58	2'X3' BOX	R-3362-L
STM INL NO. 4	1249.77	1252.86	3.09	2'X3' BOX	R-3362-L

NOTE: CURB INLET RIM ELEVATION IS TO EDGE OF PAVEMENT.



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LEGEND (PROPOSED)



GENERAL NOTES

- UNDERLYING SITE CONTOURS AND INFORMATION BASED ON TOPOGRAPHIC & UTILITY DATA AS SURVEYED BY WYSER ENGINEERING ON THE WEEK OF MAY 27, 2025. WYSER ENGINEERING SHALL NOT BE HELD RESPONSIBLE FOR ANY ERRORS OR OMISSIONS THAT MAY ARISE AS A RESULT OF ERRONEOUS OR INCOMPLETE INFORMATION PROVIDED BY OTHERS. CONTRACTOR TO CONFIRM ALL ELEVATIONS, GENERAL DRAINAGE AND EARTHWORK REQUIREMENTS PRIOR TO CONSTRUCTION.
- THE BENCHMARK LOCATIONS ARE SHOWN FOR REFERENCE ONLY ON THIS PLAN. THE BENCHMARKS SHALL BE VALIDATED BY LICENSED LAND SURVEYOR PRIOR TO CONSTRUCTION. CONTRACTOR ASSUMES RISK ASSOCIATED WITH BENCHMARK ELEVATIONS UNTIL CONFIRMED.
- CONTRACTOR TO OBTAIN APPROPRIATE PERMITS FOR STREET OPENINGS & TO WORK WITHIN THE VILLAGE'S LAND IF REQUIRED.
- WYSER ENGINEERING SHALL BE HELD HARMLESS AND DOES NOT WARRANT ANY DEVIATIONS BY THE OWNER OR CONTRACTOR FROM THE APPROVED CONSTRUCTION PLANS THAT MAY RESULT IN DISCIPLINARY ACTIONS BY REGULATORY AGENCIES.
- IF ANY ERRORS, DISCREPANCIES, OR OMISSIONS WITHIN THE PLAN BECOME APPARENT, IT SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER PRIOR TO CONSTRUCTION SO THAT CLARIFICATION OR REDESIGN MAY OCCUR.
- ALL MUNICIPAL UTILITY CONNECTIONS, WORK IN ROW, PUBLIC OUTLOTS AND PUBLIC EASEMENTS SHALL BE IN ACCORDANCE WITH VILLAGE OF MOUNT HOREB STANDARD SPECIFICATIONS.

UTILITY NOTES

- DIMENSIONS TAKE PRECEDENCE OVER SCALE. CONTRACTOR TO VERIFY ALL DIMENSIONS IN FIELD.
- LENGTHS OF ALL UTILITIES ARE TO CENTER OF STRUCTURES OR FITTINGS AND MAY VARY SLIGHTLY FROM PLAN. LENGTHS SHALL BE VERIFIED IN THE FIELD DURING CONSTRUCTION.
- CONTRACTOR SHALL VERIFY ALL ELEVATIONS, LOCATIONS, AND SIZES OF SANITARY, WATER AND STORM LATERALS AND CHECK ALL UTILITY CROSSINGS FOR CONFLICTS.
- THE PROPOSED IMPROVEMENTS MUST BE CONSTRUCTED IN ACCORDANCE WITH ENGINEERING PLANS DESIGNED TO MEET ORDINANCES AND REQUIREMENTS OF THE MUNICIPALITY AND WISDOT, WISDPS, AND WDNR.
- PRIOR TO CONSTRUCTION, THE CONTRACTOR IS RESPONSIBLE FOR:
 - EXAMINING ALL SITES CONDITIONS RELATIVE TO THE CONDITIONS INDICATED ON THE ENGINEERING DRAWINGS. ANY DISCREPANCIES ARE TO BE REPORTED TO THE ENGINEER AND RESOLVED PRIOR TO THE START OF CONSTRUCTION.
 - OBTAINING ALL PERMITS INCLUDING PERMIT COSTS, TAP FEES, METER DEPOSITS, BONDS, AND ALL OTHER FEES REQUIRED FOR PROPOSED WORK TO OBTAIN OCCUPANCY.
 - VERIFYING UTILITY ELEVATIONS AND NOTIFYING ENGINEER OF ANY DISCREPANCY. NO WORK SHALL BE PERFORMED UNTIL THE DISCREPANCY IS RESOLVED.
 - NOTIFYING ALL UTILITIES PRIOR TO THE INSTALLATION OF ANY UNDERGROUND IMPROVEMENTS.
 - NOTIFYING THE DESIGN ENGINEER AND MUNICIPALITY 48 HOURS PRIOR TO THE START OF CONSTRUCTION TO ARRANGE FOR APPROPRIATE CONSTRUCTION OBSERVATION.
- THE CONTRACTOR IS RESPONSIBLE FOR PROVIDING THE ENGINEER WITH AS-BUILT CONDITIONS OF THE DESIGNATED IMPROVEMENTS IN ORDER THAT THE APPROPRIATE DRAWINGS CAN BE PREPARED, IF REQUIRED. ANY CHANGES TO THE DRAWINGS OR ADDITIONAL ITEMS MUST BE REPORTED TO THE ENGINEER AS WORK PROGRESSES.
- ANY SANITARY SEWER, SANITARY SEWER SERVICES, WATER MAIN, WATER SERVICES, STORM SEWER, OR OTHER UTILITIES, WHICH ARE DAMAGED BY THE CONTRACTORS, SHALL BE REPAIRED TO THE OWNER'S SATISFACTION AT THE CONTRACTOR'S EXPENSE. **NO BLASTING IS ALLOWED WITHIN 30 FEET OF EXISTING UTILITIES.**
- ALL PRIVATE INTERCEPTOR WATER MAIN AND WATER SERVICES SHALL BE INSTALLED WITH A 6' MINIMUM BURY. PROVIDE INSULATION ABOVE PIPES WITH LESS THAN 5' OF GROUND COVER.
- GRANULAR BACKFILL MATERIALS ARE REQUIRED IN ALL UTILITY TRENCHES UNDER SIDEWALKS AND PROPOSED PAVED AREAS (UNLESS OTHERWISE SPECIFIED BY A GEOTECHNICAL ENGINEER). ALL UTILITY TRENCH BACKFILL SHALL BE COMPACTED PER SPECIFICATIONS. ALL PAVEMENT PATCHING SHALL COMPLY WITH THE VILLAGE OF MOUNT HOREB STANDARDS. ADDITIONAL PAVEMENT MILLING AND OVERLAY MAY BE REQUIRED BY PERMIT.

GRADING, SEEDING & RESTORATION NOTES

- ALL GRADES SHOWN ARE FINAL FINISHED SURFACE GRADES.
- AREAS TO BE SEEDED SHALL HAVE A MINIMUM 6 INCHES TOPSOIL UNLESS OTHERWISE NOTED.
- AREAS NOT RESTORED WITH EROSION MATTING OR OTHER STABILIZATION MEASURES SHALL BE STABILIZED WITH MULCH.
- APPLY ANIONIC POLYMER TO DISTURBED AREAS IF EROSION BECOMES PROBLEMATIC.
- MULCH SHALL BE WEED-FREE STRAW AND SHALL BE INSTALLED AT THE RATE OF 2 TONS PER ACRE PER SECTION 627 OF "STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION" (WISDOT 2014)
- PERMANENT SEEDING SHALL NOT OCCUR BETWEEN SEPTEMBER 15TH AND APRIL 15TH. ALTERNATE SEEDING/PLANTING METHODS AND/OR EROSION PROTECTION MAY BE NECESSARY FOR SEEDING/PLANTING THAT OCCURS DURING THAT TIME. COORDINATE WITH THE OWNER AS NECESSARY.
- TEMPORARY STABILIZATION SHALL CONSIST OF ONE OR MORE OF THE FOLLOWING OPTIONS:
 - TEMPORARY SEEDING CONSISTING OF ANNUAL RYE GRASS APPLIED AT A RATE OF 1.5 LBS PER 1000 SQUARE FEET.
 - WISDOT PAL CLASS 1 TYPE B URBAN EROSION CONTROL MAAT.

EROSION CONTROL CONSTRUCTION SCHEDULE

- 12/15/2025 - INITIAL LAND DISTURBING - INSTALL ALL PERIMETER EROSION CONTROL DEVICES INCLUDING STONE TRACKING PAD AND SILT FENCE.
- 12/15/2025 - BEGIN DEMOLITION, STRIP TOPSOIL AND STOCKPILE.
- 02/15/2025 - BUILDING CONSTRUCTION.
- DURING CONSTRUCTION
 - IF THE START DATE IS DELAYED, NOTIFIED AUTHORITIES HAVING JURISDICTION WHEN BREAKING GROUND.
 - SOIL STOCKPILES WHICH ARE LEFT UNUSED FOR MORE THAN 7 DAYS SHALL BE STABILIZED AND SILT FENCE INSTALLED AROUND THE PILE.
 - ALL SLOPES GREATER THAN 5:1 SHALL BE STABILIZED WITHIN TWO MONTHS (60 DAYS) OF BEING DISTURBED.
 - INSTALL ADDITIONAL PRACTICES AS REQUIRED BY STATE AND LOCAL INSPECTORS AND AS NECESSARY TO PREVENT RUNOFF DUE TO CONSTRUCTION MEANS AND METHODS.
 - ALL PRACTICES SHALL BE ONGOING BASED ON THE PLAN VIEW AND NOTES.
- 06/15/2026 - TEMPORARY STABILIZATION WITH MULCH AND EROSION MATTING WHERE REQUIRED OUTSIDE OF ACTIVE CONSTRUCTION AREAS.
- 08/01/2027 - SITE PAVING AND FINISH GRADING.
- 09/15/2027 - FINAL SITE STABILIZATION - ALL PVIOUS AREAS DISTURBED DURING CONSTRUCTION WILL BE RESTORED WITH A MINIMUM OF 6" OF TOPSOIL, FERTILIZER, SEED, AND MULCH WITHIN SEVEN (7) DAYS OF FINAL GRADING.

- CONTRACTOR SHALL NOTIFY THE MUNICIPAL PUBLIC WORKS DEPARTMENT A MINIMUM OF 48 HOURS BEFORE CONNECTING TO PUBLIC UTILITIES.
- ALL NON-METALLIC BUILDING SEWER AND WATER SERVICES MUST BE ACCOMPANIED BY MEANS OF LOCATING UNDERGROUND PIPE. TRACER WIRE VALVE BOXES SHALL BE INSTALLED ON ALL LATERALS AND AS INDICATED ON THESE PLANS.
- EXTERIOR CLEANOUTS SHALL BE PROVIDED AS REQUIRED BY SPS 382.35. ALL EXTERIOR CLEANOUTS SHALL BE PROVIDED WITH A FROST SLEEVE IN ACCORDANCE WITH SPS 382.34(5)(a)b AND SPS 384.30(2)(c).
- ALL PRIVATE PLUMBING MATERIALS SHALL CONFORM TO SPS 384.30.
- ALL PRIVATE PIPE JOINTS SHALL BE INSTALLED PER SPS 384.40.
- ALL PRIVATE WATER PIPE, INCLUDING DEPTH AND SEPARATION REQUIREMENTS, SHALL BE IN ACCORDANCE WITH SPS 382.40(8).
- THE CONTRACTOR SHALL ALLOW 10 WORKING DAYS FOR THE CONSTRUCTION OF GAS MAINS WHEN SCHEDULING THE WORK AND SHALL NOT RESTRICT ACCESS TO THE GAS MAIN CONTRACTOR OR OTHER UTILITY COMPANIES.
- INLET CASTINGS SHALL BE SET TO GRADE PRIOR TO AND SEPARATE FROM THE POURING OF THE CONCRETE CURB AND GUTTER. IS IS REQUIRED THAT THREE FEET OF CONCRETE CURB AND GUTTER ON EACH SIDE OF THE INLET SHALL BE POURED BY HAND, NOT THROUGH THE USE OF A CURB MACHINE. THE INLET CASTING SHALL BE SET TO GRADE ON A BED OF MORTAR WHICH SHALL BE A MINIMUM OF TWO INCHES THICK. THE INLET SHALL BE PLACED ON THE MORTAR BED AND SHALL BE ADJUSTED TO GRADE BY APPLYING DIRECT PRESSURE TO THE CASTING. ONCE THE CASTING ADJUSTMENT IS COMPLETE, THREE FEET OF CURB AND GUTTER ON EACH SIDE OF THE CASTING SHALL BE POURED BY HAND.
- CONTRACTOR SHALL VERIFY AND COORDINATE ALL UTILITY CONNECTIONS WITH THE BUILDING PRIOR TO CONSTRUCTION.
- THE CONTRACTOR SHALL CONDUCT HIS OPERATIONS SO AS TO BE IN CONFORMANCE WITH THE VILLAGE EROSION CONTROL AND STORMWATER ORDINANCE, AND DNR ADMINISTRATIVE RULE NR 216 AT ALL TIMES.

CONSTRUCTION SITE EROSION CONTROL REQUIREMENTS

- POST MUNICIPAL EROSION CONTROL PERMITS ON SITE AND MAINTAIN UNTIL CONSTRUCTION ACTIVITIES HAVE CEASED, THE SITE IS STABILIZED, AND A NOTICE OF TERMINATION IS FILED WITH MUNICIPALITY.
- KEEP A COPY OF THE CURRENT EROSION CONTROL PLAN ON SITE THROUGHOUT THE DURATION OF THE PROJECT.
- ENGINEER / VILLAGE OF MOUNT HOREB HAS THE RIGHT TO REQUIRE CONTRACTOR TO IMPLEMENT ADDITIONAL EROSION CONTROL MEASURES AS NECESSARY. CONTRACTOR MUST NOTIFY THE VILLAGE OF MOUNT HOREB BUILDING INSPECTOR TWO (2) WORKING DAYS IN ADVANCE OF ANY SOIL DISTURBANCE ACTIVITIES.
- SUBMIT PLAN REVISIONS OR AMENDMENTS TO THE VILLAGE AT LEAST 5 DAYS PRIOR TO FIELD IMPLEMENTATION.
- THE SITE CONTRACTOR IS RESPONSIBLE FOR ROUTINE SITE INSPECTIONS AT LEAST ONCE EVERY 7 DAYS AND WITHIN 24 HOURS AFTER A RAINFALL EVENT OF 0.5 INCHES OR GREATER. KEEP INSPECTION REPORTS ON-SITE AND MAKE THEM AVAILABLE UPON REQUEST.
- INSPECT AND MAINTAIN ALL INSTALLED EROSION CONTROL PRACTICES UNTIL THE CONTRIBUTING DRAINAGE AREA HAS BEEN STABILIZED.
- WHEN POSSIBLE: PRESERVE EXISTING VEGETATION (ESPECIALLY ADJACENT TO SURFACE WATERS), MINIMIZE LAND-DISTURBING CONSTRUCTION ACTIVITY ON SLOPES OF 20% OR MORE, MINIMIZE SOIL COMPACTION, AND PRESERVE TOPSOIL.
- REFER TO THE WDNR STORMWATER CONSTRUCTION TECHNICAL STANDARDS AT http://dnr.wi.gov/topic/stormwater/standards/const_standards.html.
- INSTALL PERIMETER EROSION CONTROLS AND ROCK TRACKING PAD CONSTRUCTION ENTRANCE(S) PRIOR TO ANY LAND-DISTURBING ACTIVITIES, INCLUDING CLEARING AND GRUBBING. USE WDNR TECHNICAL STANDARD STONE TRACKING PAD AND TIRE WASHING #1057 FOR ROCK CONSTRUCTION ENTRANCE(S).
- INSTALL INLET PROTECTION PRIOR TO LAND-DISTURBING ACTIVITIES IN THE CONTRIBUTING DRAINAGE AREA AND/OR IMMEDIATELY UPON INLET INSTALLATION. COMPLY WITH WDNR TECHNICAL STANDARD STORM DRAIN INLET PROTECTION FOR CONSTRUCTION SITES #1060 AND DANE COUNTY REQUIREMENTS FOR FRAMED INLET PROTECTION.
- CONTRACTOR TO PROVIDE SOLID LID OR METAL PLATE ON ALL OPEN MANHOLES DURING CONSTRUCTION TO MINIMIZE SEDIMENT FROM ENTERING THE STORM SEWER SYSTEM.
- STAGE CONSTRUCTION GRADING ACTIVITIES TO MINIMIZE THE CUMULATIVE EXPOSED AREA, CONDUCT TEMPORARY GRADING FOR EROSION CONTROL PER WDNR TECHNICAL STANDARD TEMPORARY GRADING PRACTICES FOR EROSION CONTROL #1067.
- PERMITTING OF GROUNDWATER DEWATERING IS THE RESPONSIBILITY OF THE CONTRACTOR. GROUNDWATER DEWATERING IS SUBJECT TO A DNR WASTEWATER DISCHARGE PERMIT AND A DNR HIGH CAPACITY WELL APPROVAL IF CUMULATIVE PUMP CAPACITY IS 70 GPM OR MORE.
- PROVIDE ANTI-SCOUR PROTECTION AND MAINTAIN NON-EROSIVE FLOW DURING DEWATERING. PERFORM DEWATERING OF ACCUMULATED SURFACE RUNOFF IN ACCORDANCE WITH WDNR TECHNICAL STANDARD DE-WATERING #1061.
- INSTALL AND MAINTAIN SILT FENCING PER WDNR TECHNICAL STANDARD SILT FENCE #1056. REMOVE SEDIMENT FROM BEHIND SILT FENCES AND SEDIMENT BARRIERS BEFORE SEDIMENT REACHES A DEPTH THAT IS EQUAL TO ONE-HALF OF THE FENCE AND/OR BARRIER HEIGHT.

- REPAIR BREAKS AND GAPS IN SILT FENCES AND BARRIERS IMMEDIATELY. REPLACE DECOMPOSING STRAW BALES (TYPICAL BALE LIFE IS 3 MONTHS), LOCATE, INSTALL, AND MAINTAIN STRAW BALES PER WDNR TECHNICAL STANDARD DITCH CHECKS #1062.
- INSTALL AND MAINTAIN FILTER SOCKS IN ACCORDANCE WITH WDNR TECHNICAL STANDARD INTERIM MANUFACTURED PERIMETER CONTROL AND SLOPE INTERRUPTION PRODUCTS # 1071.
- IMMEDIATELY STABILIZE STOCKPILES AND SURROUND STOCKPILES AS NEEDED WITH SILT FENCE OR OTHER PERIMETER CONTROL IF STOCKPILES WILL REMAIN INACTIVE FOR 7 DAYS OR LONGER.
- IMMEDIATELY STABILIZE ALL DISTURBED AREAS THAT WILL REMAIN INACTIVE FOR 14 DAYS OR LONGER. BETWEEN SEPTEMBER 15 AND OCTOBER 15: STABILIZE WITH MULCH, TACKIFIER, AND A PERENNIAL SEED MIXED WITH WINTER WHEAT, ANNUAL OATS, OR ANNUAL RYE. AS APPROPRIATE FOR REGION AND SOIL TYPE. OCTOBER 15 THROUGH COLD WEATHER: STABILIZE WITH A POLYMER AND DORMANT SEED MIX, AS APPROPRIATE FOR REGION AND SOIL TYPE.
- STABILIZE AREAS OF FINAL GRADING WITHIN 7 DAYS OF REACHING FINAL GRADE.
- SWEEP/CLEAN UP ALL SEDIMENT/TRASH THAT MOVES OFF-SITE DUE TO CONSTRUCTION ACTIVITY OR STORM EVENTS BEFORE THE END OF THE SAME WORKDAY OR AS DIRECTED BY THE AUTHORITIES WITH JURISDICTION. SEPARATE SWEEPED MATERIALS (SOILS AND TRASH) AND DISPOSE OF APPROPRIATELY.
- THE CONTRACTOR IS RESPONSIBLE FOR CONTROLLING DUST PER WDNR TECHNICAL STANDARD DUST CONTROL ON CONSTRUCTION SITES # 1068.
- PROPERLY DISPOSE OF ALL WASTE AND UNUSED BUILDING MATERIALS (INCLUDING GARBAGE, DEBRIS, CLEANING WASTES, OR OTHER CONSTRUCTION MATERIALS) AND DO NOT ALLOW THESE MATERIALS TO BE CARRIED BY RUNOFF INTO THE RECEIVING CHANNEL.
- COORDINATE WITH THE AUTHORITIES WITH JURISDICTION TO UPDATE THE LAND DISTURBANCE PERMIT TO INDICATE THE ANTICIPATED OR LIKELY DISPOSAL LOCATIONS FOR ANY EXCAVATED SOILS OR CONSTRUCTION DEBRIS THAT WILL BE HAULED OFF-SITE FOR DISPOSAL. THE DEPOSITED OR STOCKPILED MATERIAL NEEDS TO INCLUDE PERIMETER SEDIMENT CONTROL MEASURES (SUCH AS SILT FENCE, HAY BALES, FILTER SOCKS, OR COMPACTED EARTHEN BERMS).
- FOR NON-CHANNELIZED FLOW ON DISTURBED OR CONSTRUCTED SLOPES, PROVIDE CLASS CLASS 1 TYPE B EROSION CONTROL MATTING. INSTALL AND MAINTAIN PER WDNR TECHNICAL STANDARD NON-CHANNEL EROSION MAT #1052.
- FOR CHANNELIZED FLOW ON DISTURBED OR CONSTRUCTED AREAS, PROVIDE CLASS II TYPE B EROSION CONTROL MATTING UNLESS OTHERWISE SPECIFIED ON THE PLAN. INSTALL AND MAINTAIN PER WDNR TECHNICAL STANDARD CHANNEL EROSION MAT #1053.
- MAKE PROVISIONS FOR WATERING DURING THE FIRST 8 WEEKS FOLLOWING SEEDING OR PLANTING OF DISTURBED AREAS WHENEVER MORE THAN 7 CONSECUTIVE DAYS OF DRY WEATHER OCCUR.
- THE CONTRACTOR IS RESPONSIBLE FOR COMPLYING WITH ALL APPLICABLE WDNR REMEDIATION AND WASTE MANAGEMENT REQUIREMENTS FOR HANDLING AND DISPOSING OF CONTAMINATED MATERIALS. SITE-SPECIFIC INFORMATION FOR AREAS WITH KNOWN OR SUSPECTED SOIL AND/OR GROUNDWATER CONTAMINATION CAN BE FOUND ON WDNR'S BUREAU OF REMEDIATION AND REDEVELOPMENT RACKING SYSTEM (BRRTS) PUBLIC DATABASE AT: <http://dnr.wi.gov/botw/>
- INSTALL AND MAINTAIN A CONCRETE WASHOUT BASIN PER EPA 833-F-11-006: <https://www3.epa.gov/npdes/pubs/concretewashout.pdf> REQUIRE USE BY ALL CONCRETE CONTRACTORS. LIQUID MAY BE REUSED IN CONCRETE MIXING, EVAPORATED, OR DISPOSED OF AS WASTEWATER.

LANGHUS ON MAIN

VILLAGE OF MOUNT HOREB, DANE COUNTY, WI

Sheet Title:
NOTES AND LEGEND

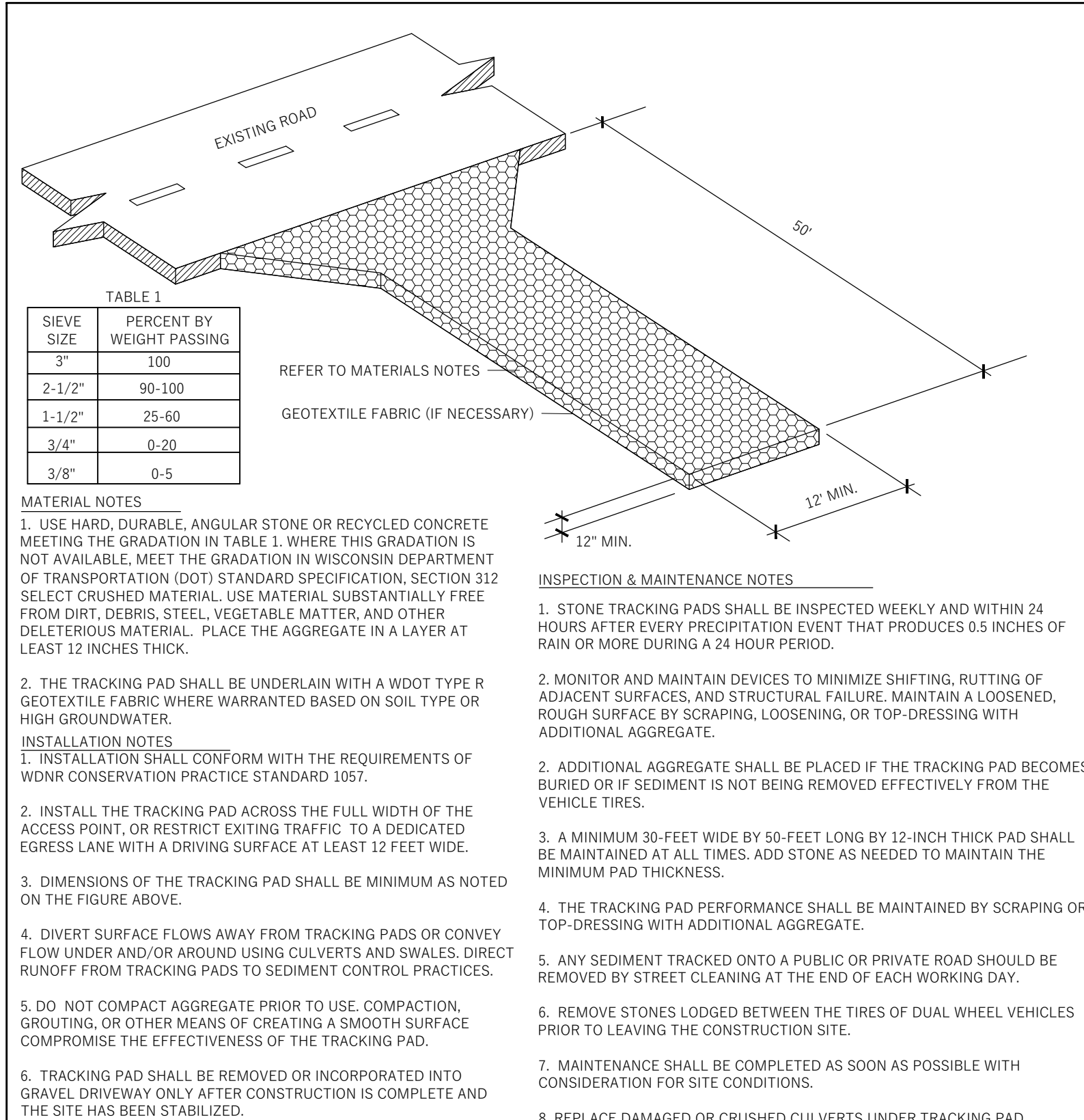
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ENGINEERING

210-214 E MAIN STREET
MOUNT HOREB, WI 53572

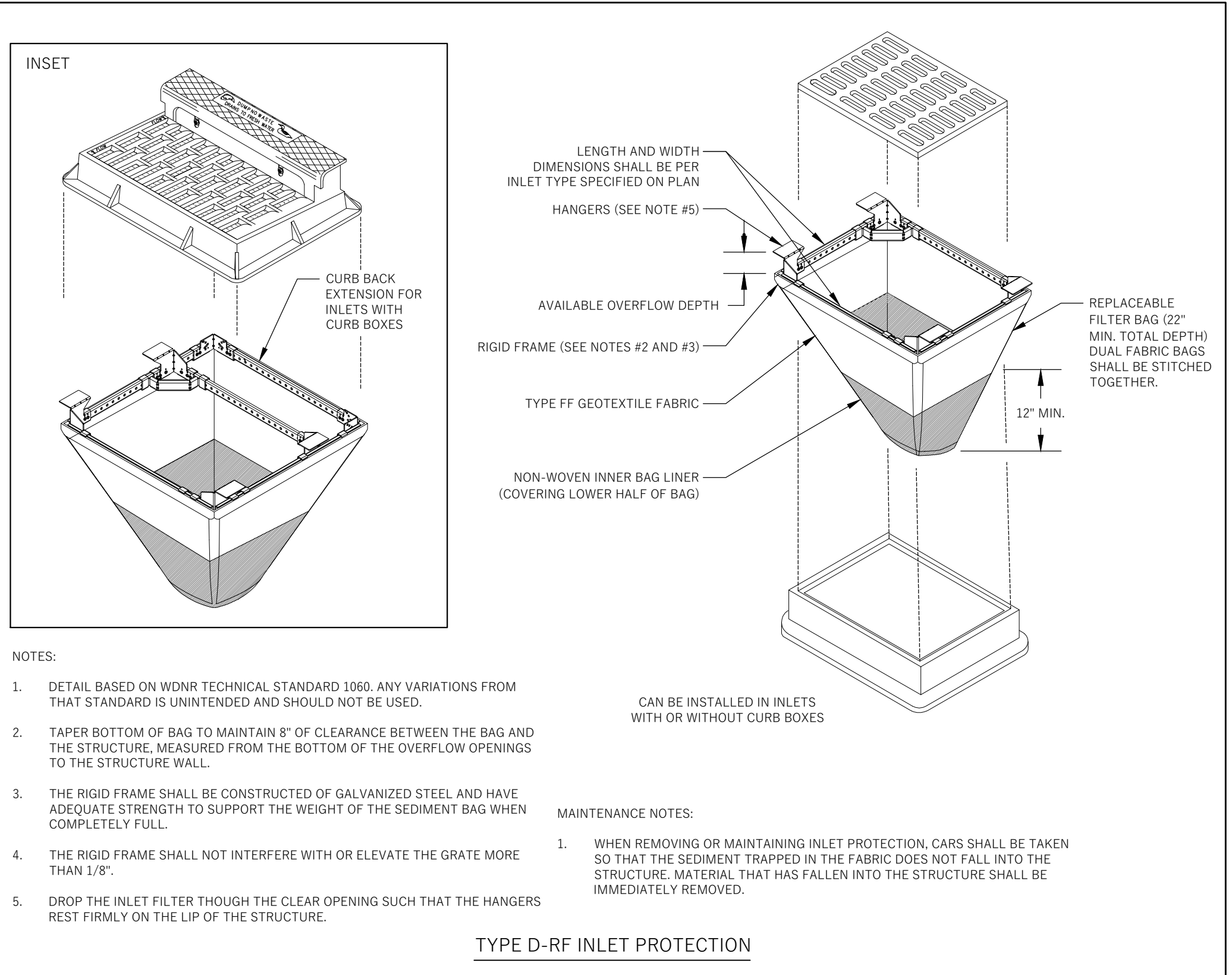
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No.	Date:	Description:

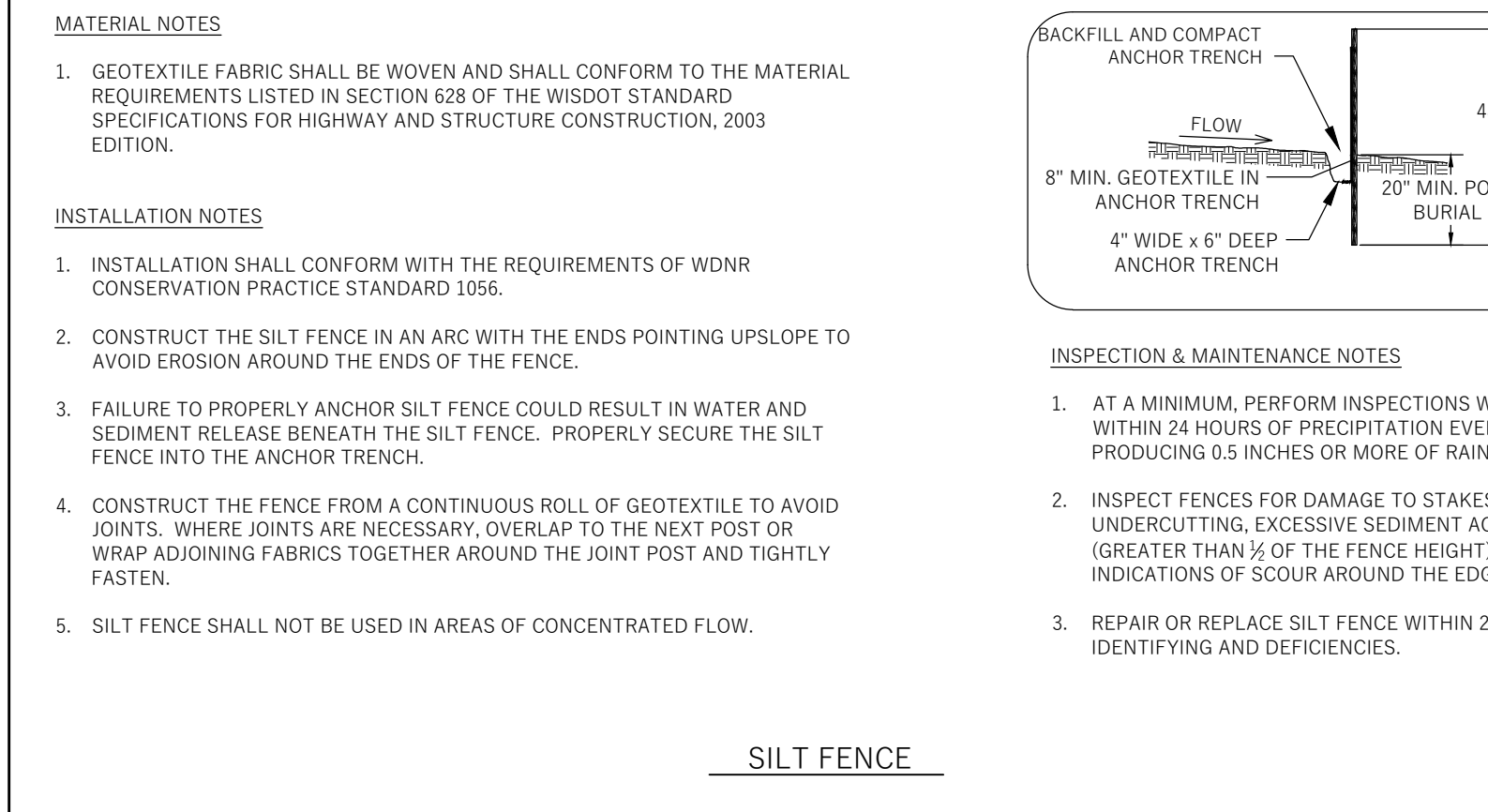
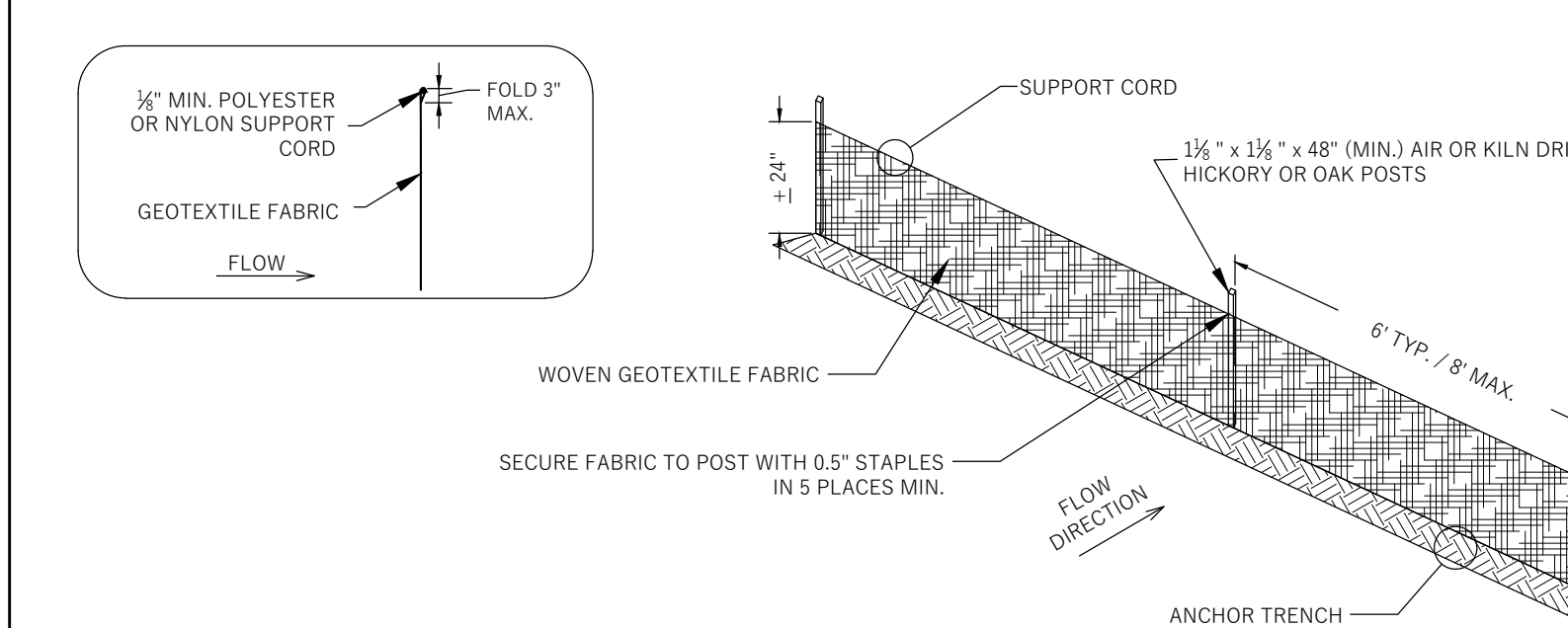
Graphic Scale	
Wyser Number	25-1430
Set Type	SIP
Date Issued	11/14/2025
Sheet Number	C400



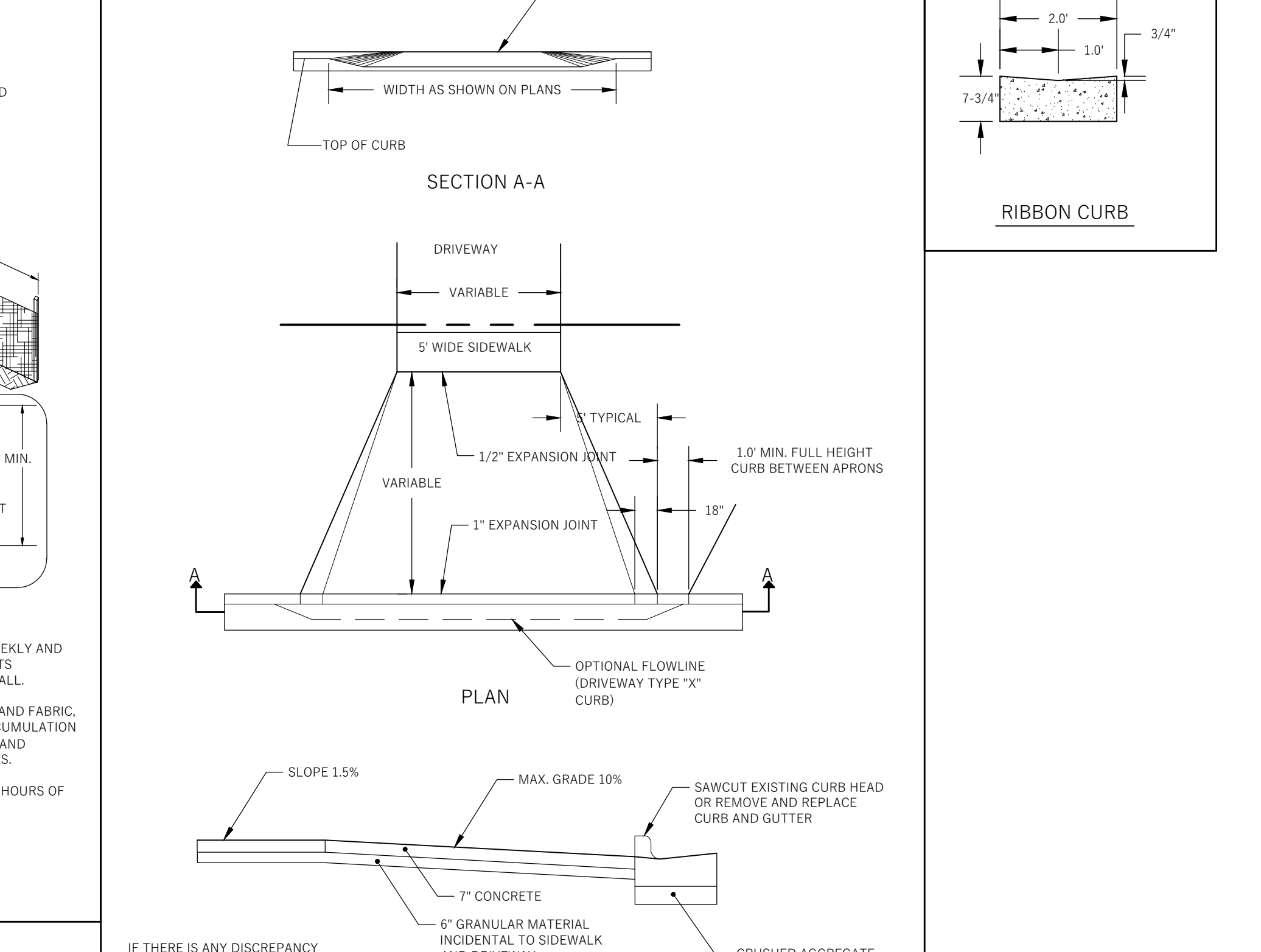
CONSTRUCTION ENTRANCE (STONE TRACKING PAD)



TYPE D-RF INLET PROTECTION



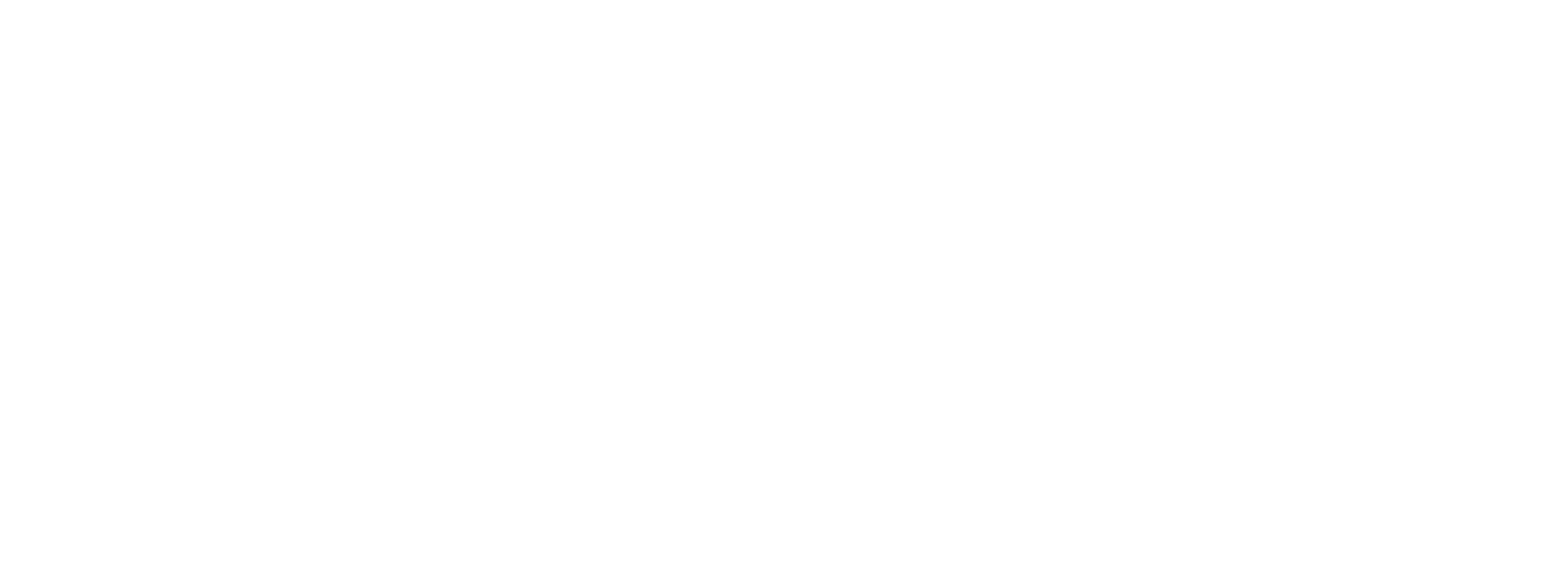
SILT FENCE



COMMERCIAL ENTRANCE



RIBBON CURB



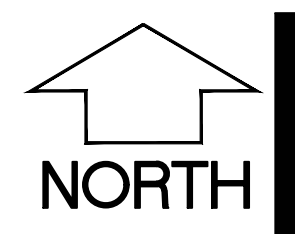
COMMERCIAL ENTRANCE

Revisions:

No.	Date:	Description:

Graphic Scale	
Wyser Number	25-1430
Set Type	SIP
Date Issued	11/14/2025
Sheet Number	C401

LEGEND (PROPOSED)
 - - - - - PROPOSED PROPERTY BOUNDARY
 - - - - - EASEMENT
 - - - - - BUILDING FOOTPRINT
 - - - - - CONCRETE PAVEMENT



PUBLIC ALLEY
 16.5' RIGHT-OF-WAY

OVERALL PARCEL
 31,653 SQ. FT.
 OR 0.73 ACRES

PROPOSED BUILDING
 28,500 SQ.FT.

GREEN ROOF OVER CANOPY STRUCTURE ("LIVEROOF" OR SIM.)

PL1: +/- 36" TALL PLANTER WITH VERTICAL ANNUALS (5 THUS)

NORTH THIRD STREET

2.5' WIDE PUBLIC SIDEWALK EASEMENT.

2.5' WIDE PUBLIC SIDEWALK EASEMENT.

4.5' WIDE PUBLIC SIDEWALK EASEMENT.

PL2: +/- 30" TALL PLANTER WITH VERTICAL ANNUALS (8 THUS)

EXISTING STREET TREE TO REMAIN PROTECT DURING CONSTRUCTION

EXISTING STREET TREE TO REMAIN PROTECT DURING CONSTRUCTION

GREEN ROOF OVER CANOPY STRUCTURE ("LIVEROOF" OR SIM.)

EXISTING STREET TREE TO REMAIN PROTECT DURING CONSTRUCTION

EXISTING STREET TREE TO REMAIN PROTECT DURING CONSTRUCTION

EXISTING STREET TREE TO REMAIN PROTECT DURING CONSTRUCTION

EAST MAIN STREET

LANGHUS ON MAIN

VILLAGE OF MOUNT HOREB, DANE COUNTY, WI

210-214 E MAIN STREET
 MOUNT HOREB, WI 53572

Sheet Title:
 LANDSCAPE PLAN

Revisions:
 No. Date Description

Graphic Scale: 0' 5' 10' 15'

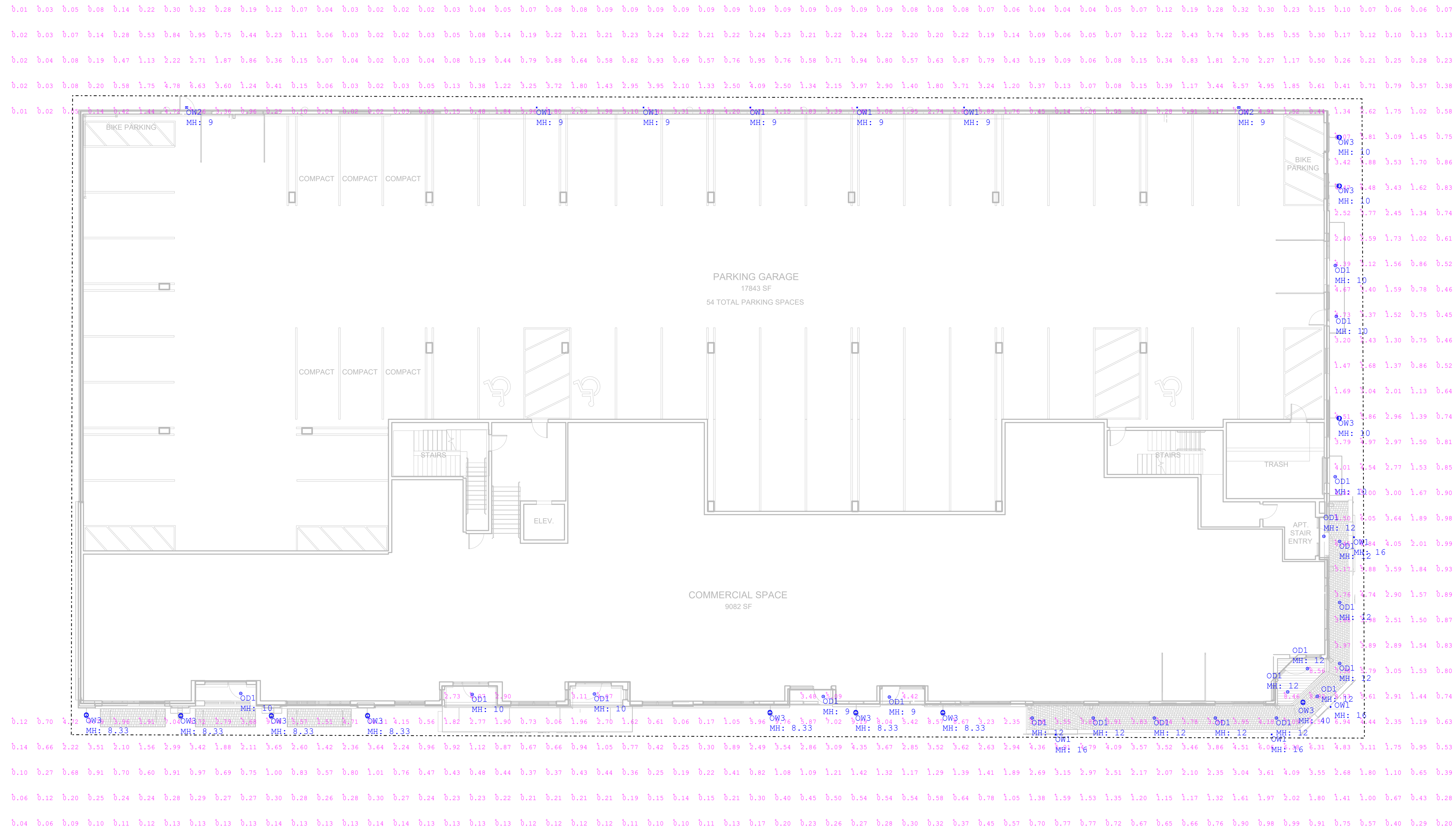
Wyser Number: 25-1430
 Set Type: SIP

Date Issued: 11/14/2025

Sheet Number: **L100**

NOTE: ALL PLANTERS PROPOSED SHALL BE MOBILE AND PLACED OUTSIDE OF THE PUBLIC RIGHT OF WAY AND ANY SIDEWALK EASEMENT

File: W:\2025\251430_JG_Development - Oloans Development, Mount Horeb\DWG\251430_Civil_Designing.dwg Layout: Landscape Plan User: jolayr Plotted: Nov 13, 2025 - 3:13pm



Qty	Label	LLF	MFG	Description	Lum. Watts	Total Watts	Lum. Lumens
20	OD1	0.950	TGS	880715-R3-SCCT	15	300	1013
9	OW1	0.950	KUZCO	EW46308-BK-UNV-3CCT	20	180	1120
2	OW2	0.950	LITHONIA	WPX0 LED ALO SWW2 MVOLT PE DDBXD M2	124511	24902	1645
11	OW3	0.950	ANP	A810 AM01W D xxK (finish) E25	1323	145.53	1207

Label	CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min
SITE	Illuminance	Fc	1.68	10.11	0.01	168.00	1011

- NOTES:
- Customers are responsible for confirming mounting heights, fixture suspension types/ lengths, color temperature, CRI, linear fixture lengths, pole lengths, and bollard heights/ lengths prior to ordering.
 - Mounting height (MH) is measured from the bottom of the fixture to the floor.
 - This Lighting layout assumes the following unless values are specified and must be confirmed by the customer prior to ordering.
 - Room reflectance of 80, 50, 20 for standard ceilings and 50, 50, 20 for exposed ceilings
 - Wall sconces are mounted at 7' for calculation purposes. Customer must confirm desired mounting height before rough in.

#	DATE	COMMENTS
REVISIONS		
DRAWN BY : JT		
DATE : NOV 12, 2025		
SCALE : 3/32" = 1'		
LANGUS MIXED USE	MOUNT HOREB, WISCONSIN	SITE LIGHTING LAYOUT



Enterprise Lighting Ltd
Manufacturers' Representative

Date: Nov 13, 2025

Enterprise Lighting, LTD.
2007 Pewaukee Rd.
Waukesha WI 53188
Phone: (262) 953-2700

Job Name
Langus Mixed Use
ELL25-143233
Mount Horeb WI

Bid Date
Nov 12, 2025

Submittal Date
Nov 13, 2025



Enterprise Lighting Ltd
Manufacturers' Representative

Transmittal

Enterprise Lighting, LTD.
2007 Pewaukee Rd.
Waukesha WI 53188
Phone: (262) 953-2700
From: Andrea Goral

Project Langus Mixed Use
Quote# ELL25-143233
Location Mount Horeb WI
To Electrical Solutions
2928 HWY MN
Stoughton WI 53589
Contact: Bernie Esch

ATTACHED WE ARE SENDING YOU 1 COPY OF THE FOLLOWING ITEM:

- | | | |
|--|---|--------|
| <input checked="" type="checkbox"/> Drawings | <input type="checkbox"/> Specifications | Other: |
| <input type="checkbox"/> Prints | <input type="checkbox"/> Information | |
| <input type="checkbox"/> Plans | <input type="checkbox"/> Submittals | |

THESE ARE TRANSMITTED FOR:

- | | | |
|--|---|---------------------------------|
| <input type="checkbox"/> Prior Approval | <input type="checkbox"/> Resubmittal for Approval | <input type="checkbox"/> Record |
| <input checked="" type="checkbox"/> Approval | <input type="checkbox"/> Corrections | Bids due on: |
| <input type="checkbox"/> Approval as Submitted | <input type="checkbox"/> Your Use | Other: |
| <input type="checkbox"/> Approval as Noted | <input type="checkbox"/> Review and Comment | |

Type	MFG	Part
OD1	Truly Green Solutions, Inc.	880715-R3-5CCT
OW1	Kuzco Lighting	EW46308-BK-UNV-3CCT
OW2	Lithonia Exterior	WPX0 LED ALO SWW2 MVOLT PE DDBXD M2
OW3	ANP Lighting	A810-AM10W-D-**K-E25



Job Name:
Langus Mixed Use

Catalog Number:
880715-R3-5CCT

Notes:

Type:

OD1

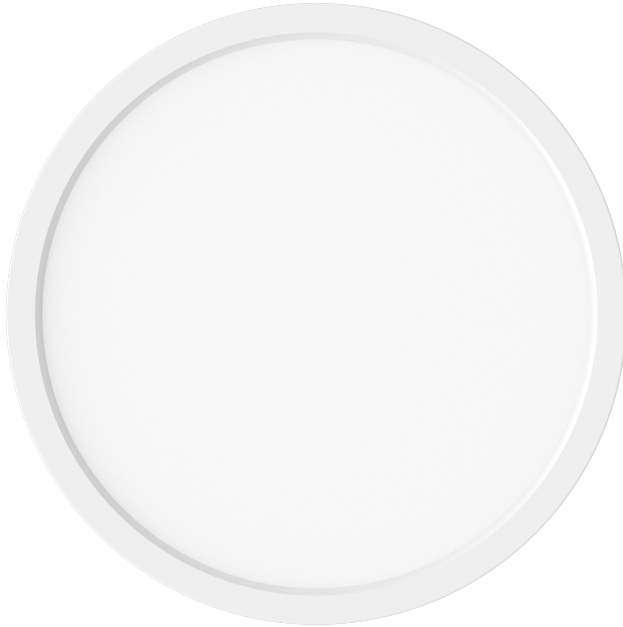
ELL25-143233

INDOOR

tgs

Date:	
Project Name:	
SKU #:	

ThinTek™ Mini R3
Surface Mount Downlight



APPLICATIONS

Residential, Commercial, Multi-Family

FEATURES

Construction

- Die-cast Aluminum Housing

Finish

- White Trim [Standard]
 - Brushed Nickel Trim
 - Black Trim
 - Bronze Trim
- [Trim Options Sold Separately]

Electrical & Technical

- Input Voltage: 120V
- Input Frequency: 60Hz
- Projected L70: ≥ 50,000 hrs
- PF: >0.9
- CRI: >90
- THD: <20%
- CCT Selectable: 2700K/3000K/
3500K/4000K/5000K
- Operating Temp.: -4°F ~ 113°F
- Wet Location Rated

Optical

- Edge-lit
- 120° Beam Angle

Installation/Mounting

- Surface Mounted:
2.75" and 3.5" Junction Box

Controls

- Triac Dimming

Compliance

- JA8 TITLE 24 Compliant

Dimmer Compatibility List:

Leviton: 6672, 6674, DSL06, RNL06, IPL06
Lutron: TG-600PR-WH, S-600PR-WH, CTCL-153PDH-WH, TGCL-153PH-LA, SLV-600P-AL, DV-600PR-WH, SCL-153PH-WH, GT-600P, DVCL-153P, MSCL-OP153M, MACL-153M//MA-R, PD-6WCL

Lumen Output

- 5"/10W: 710lm
- 7"/15W: 1,241lm
- 9"/18W: 1,460lm
- 11"/21W: 2,156lm
- 13"/24W: 2,431lm

Warranty

- 5 Year Warranty
- See warranty documentation for more information.



ORDERING INFORMATION

EXAMPLE: 880510-R3-5CCT

88			
Round Panel	Size/Wattage	Shape/Type	CCT
88	0510 - 5"/10W 0715 - 7"/15W 0918 - 9"/18W 1121 - 11"/21W 1324 - 13"/24W	R3 - Round	5CCT - 2700K/3000K/3500K/4000K/5000K

Trim Finish Options	
8805-R3 (5" Trim)	BK (Black)
8807-R3 (7" Trim)	BN (Brushed Nickel)
8809-R3 (9" Trim)	BZ (Bronze)
8811-R3 (11" Trim)	
8813-R3 (13" Trim)	

[Choose Trim Size and Finish Example: 8805-R3-BK]



www.trulygreensolutions.com
06232025

9601 Variel Ave, Chatsworth, CA 91311
(818) - 206 - 4404

Specifications & dimensions subject to change without notice.
*MOQ & longer lead times may apply, please contact customer service for more information.

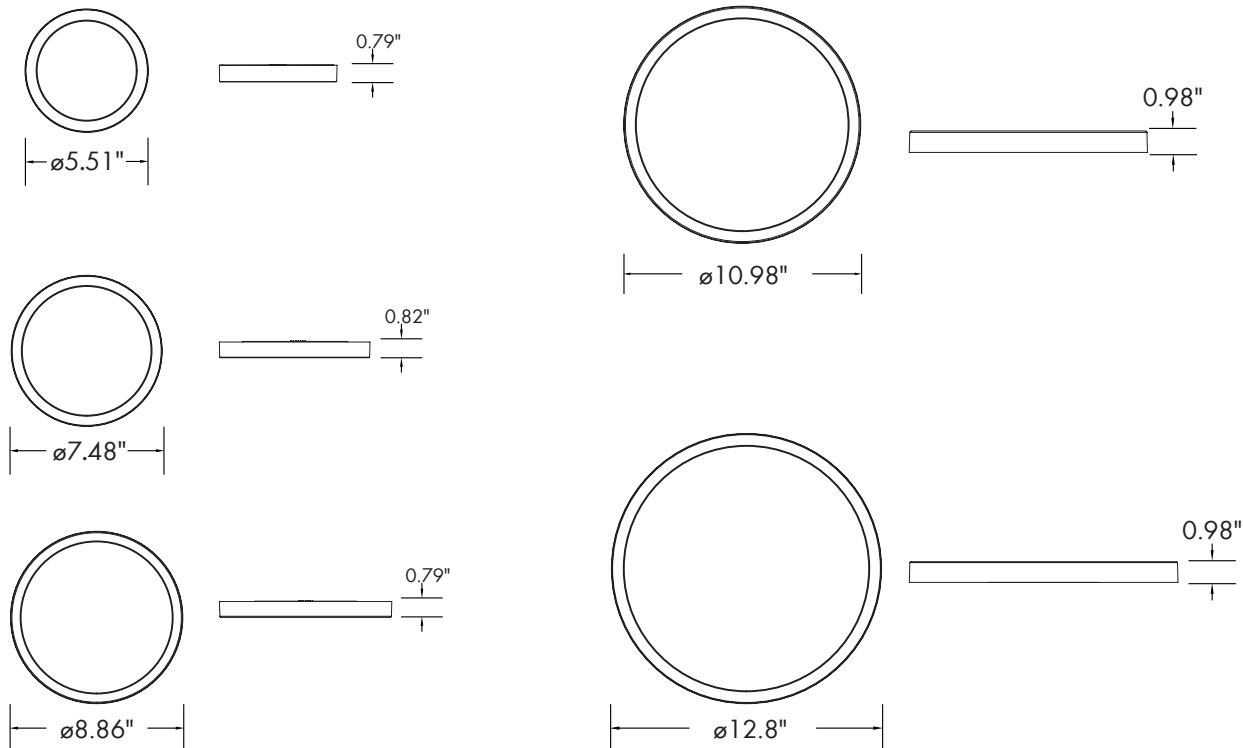


INDOOR

tgs

ThinTek™ Mini R3
Surface Mount Downlight

DIMENSIONS



LUMEN OUTPUT

SKU	Size (in)	Wattage (W)	(2700K)		(3000K)		(3500K)		(4000K)		(5000K)	
			Delivered Lumens (lm)	Efficacy (lm/W)	Delivered Lumens (lm)	Efficacy (lm/W)	Delivered Lumens (lm)	Efficacy (lm/W)	Delivered Lumens (lm)	Efficacy (lm/W)	Delivered Lumens (lm)	Efficacy (lm/W)
880510-R3-5CCT	5	10	668	66.8	671	67.1	693	69.3	710	71.0	688	68.8
880715-R3-5CCT	7	15	1186	79.06	1201	80.06	1223	84.53	1241	82.7	1215	81
880918-R3-5CCT	9	18	1389	77.16	1409	78.27	1437	79.83	1460	81.11	1426	65
881121-R3-5CCT	11	21	2016	96	2037	97	2079	99	2156	102.66	2108	100.38
881324-R3-5CCT	13	24	2386	99.41	2405	100.20	2427	101.12	2431	101.29	2419	100.79

ACCESSORIES



Mounting Bracket
(Included)



Brushed Nickel Trim
[5", 7", 9", 11", 13"]
(Option)
[BN]



Black Trim
[5", 7", 9", 11", 13"]
(Option)
[BK]



Bronze Trim
[5", 7", 9", 11", 13"]
(Option)
[BZ]



LENOX

PROJECT

EW46308-UNV-3CCT

WALL



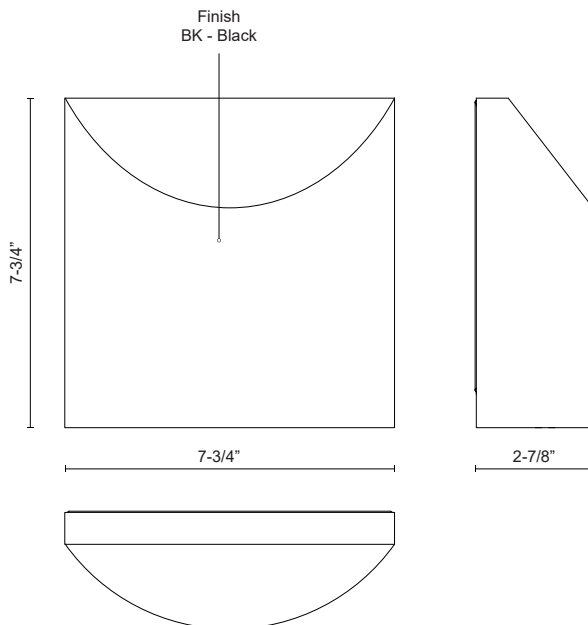
DESCRIPTION

High power LED exterior wall mount fixture intended for use as a downlight. Lenox's die-cast aluminum housing is 8-inch square in its front view, but as the viewer moves to the side the fixture's profile becomes a gently swept curve suggestive of a water drop or perhaps a ship's sail filling with wind. Lenox aimed downwards through its polymeric diffuser lights both the mounting plane and floor. Lenox is available with black finish.

SPECIFICATION DETAILS

Fixture Dimensions	W7-3/4" x H7-3/4" x E2-7/8"
Height from center	3-7/8"
Light Source	LED with DC Driver
Wattage	20W
Total Lumens	1680lm
Delivered Lumens	BK-1060lm*
Voltage	120-277V
Color Temperature	Selectable CCT 3000K/4000K/5000K
CRI (Ra)	90CRI
LED Rated Life	50,000 hours
Dimming	100% - 10%, TRIAC or ELV Dimmer (Not Included)
Glass Details	Clear Glass
ADA Compliant	Yes
Location	Wet, IP65
Illumination Direction	Down
Mounting Style	Wall Mount, Down Only
Material	Aluminum + Glass
Paint Finish	BK02

* For custom options, consult factory for details.
* For warranty information, please visit www.kuzcolighting.com/warranty



KUZCO

CANADA: 19054 28TH AVENUE - SURREY, BC V3Z 6M3
USA: 3035 E. LONE MOUNTAIN ROAD - LAS VEGAS, NV 89081

WWW.KUZCOLIGHTING.COM
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COMMENT





Catalog Number
Notes
Type

Contractor Select™ WPX LED

Wall packs

The WPX LED wall packs are energy-efficient, cost-effective, and aesthetically appealing full-cut off solution for both new construction and HID wall pack replacement/renovation opportunities. Reliable IP66 construction and excellent LED lumen maintenance ensure a long service life.

FEATURES:

- Architectural design at very economical prices
- Energy efficient - payback in less than two years
- Wide range of configuration options available

Note : WPX3 lumen package and all the WPX configuration options are not included in the Contractor Select program. For more information, please visit [WPX LED](#).



Luminaire	CCT	Lumens	Input Watts	Photocell	Finish	Voltage	Catalog Number	CI Code	UPC	Pallet qty.	Replaces Up To
WPX0	SWW2 3000K/ 4000K/ 5000K	850 - 1,650	6.4-13W	Switchable On/Off	DARK BRONZE	120-277V	WPX0 LED ALO SWW2 MVOLT PE DDBXD M2	*276U4U	196182511806	280	70W Metal Halide
WPX1	4000K	2,900	24W	N/A	DARK BRONZE	120-277V	WPX1 LED P2 40K MVOLT DDBXD M4	*265SWK	193048870589	160	150W Metal Halide
WPX2	4000K	6,000	47W	N/A	DARK BRONZE	120-277V	WPX2 LED 40K MVOLT DDBXD M2	*265SX3	193048870756	120	250W Metal Halide
	5000K	6,000	47W	N/A	DARK BRONZE	120-277V	WPX2 LED 50K MVOLT DDBXD M2	*265SX6	193048870770	120	250W Metal Halide

More configurations are available. [Click here](#) or visit www.acuitybrands.com and search for [WPX LED](#).



Job Name:
Langus Mixed Use

Catalog Number:
WPX0 LED ALO SWW2 MVOLT PE
DDBXD M2
Notes:

Type:

OW2

ELL25-143233



Specifications

INTENDED USE:

The WPX LED wall packs are designed to provide a cost-effective, energy-efficient solution for the one-for-one replacement of existing HID wall packs. The WPX0, WPX1, WPX2 and WPX3 are ideal for replacing up to 70W, 150W, 250W, and 400W HID luminaires respectively. WPX luminaires deliver a uniform, wide distribution. WPX is rated for -40°C to 40°C.

CONSTRUCTION:

WPX feature a die-cast aluminum main body with optimal thermal management that both enhances LED efficacy and extends component life. The luminaires are IP66 rated, and sealed against moisture or environmental contaminants.

ELECTRICAL:

Light engine(s) configurations consist of high-efficacy LEDs with a min LED lumen maintenance of L86/100,000 hours. Color temperature (CCT) options of 3000K, 4000K and 5000K with minimum CRI of 70 (80 for WPX0). Electronic drivers ensure system power factor >90% and THD <20%. All luminaires have 6kV surge protection (Note: WPX1 LED P1 package and WPX0 comes with a standard surge protection rating of 2.5kV).

INSTALLATION:

WPX can be mounted directly over a standard electrical junction box. A port on the back surface allows poke-through conduit wiring on surfaces that don't have an electrical junction box. WPX1, WPX2 and WPX3 come with three 1/2 inch conduit ports on three sides that allow for surface conduit wiring. Wiring can be made in the integral wiring compartment in all cases. WPX is only recommended for installations with LEDs facing downwards.

LISTINGS:

CSA Certified to meet U.S. and Canadian standards. Suitable for wet locations. IP66 Rated. DesignLights Consortium® (DLC) Premium qualified product and DLC qualified product. Not all versions of this product may be DLC Premium qualified or DLC qualified. Please check the DLC Qualified Products List at www.designlights.org/QPL to confirm which versions are qualified.

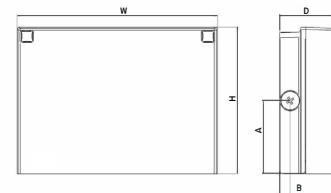
WARRANTY:

5-year limited warranty. This is the only warranty provided and no other statements in this specification sheet create any warranty of any kind. All other express and implied warranties are disclaimed. Complete warranty terms located at: www.acuitybrands.com/support/warranty/terms-and-conditions

Note: Actual performance may differ as a result of end-user environment and application. All values are design or typical values, measured under laboratory conditions at 25°C. Specifications subject to change without notice.

Dimensions

All dimensions are inches (centimeters) unless otherwise indicated.



Front View

Side View

Luminaire	Height (H)	Width (W)	Depth (D)	Side Conduit Location		Weight
				A	B	
WPX0	5.75" (14.6 cm)	5.5" (14.0 cm)	2" (5.1 cm)	N/A	N/A	2.5 lbs (1.1 kg)
WPX1	8.1" (20.6 cm)	11.1" (28.3 cm)	3.2" (8.1 cm)	4.0" (10.3 cm)	0.6" (1.6 cm)	6.1 lbs (2.8 kg)
WPX2	9.1" (23.1 cm)	12.3" (31.1 cm)	4.1" (10.5 cm)	4.5" (11.5 cm)	0.7" (1.7 cm)	8.2 lbs (3.7 kg)
WPX3	9.5" (24.1 cm)	13.0" (33.0 cm)	5.5" (13.7 cm)	4.7" (12.0 cm)	0.7" (1.7 cm)	11.0 lbs (5.0 kg)



Job Name:
Langus Mixed Use

Catalog Number:
A810-AM10W-D-**K-E25

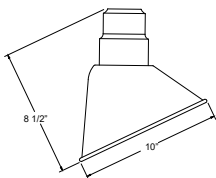
Notes:

Type:
OW3

ELL25-143233

ANP Lighting

Specifications A810



Specifications

Material:
RLM shades are constructed of 12 gauge 1100 series spun aluminum. Wall back plate and driver housing are cast aluminum, some types include a spun metal cover. All fasteners are stainless steel. Some washers may be galvanized. Inside of shade is reflective white finish for all colors except galvanized paint finish. Screw hardware may not match paint.

Drivers:
Universal voltage 120-277 is standard. 0-10V, TRIAC and ELV dimming to 1% protocols are standard for LED modules. (Refer to Driver Specification section for other specifications.)

Electrical:
Approximately 6" of pull wire extends from luminaire. All luminaires are prewired with 0-10V dimming leads. Post Arms and Wall Mounts are also prewired with line voltage & dimming leads.

Project Name: _____
Fixture Type: _____ **Quantity:** _____
Customer: _____

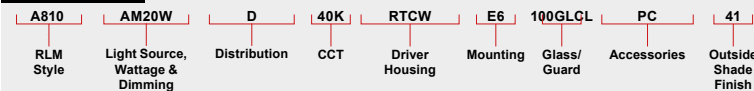
Certifications:
Arm mount, stem mount and wall mount are UL Listed for wet locations. LED Module is IP66.

Finish:
A polyester powder coat high quality finish. Marine grade finish is available.

Modifications:
Consult factory for custom or modified designs.

Buy American:
This product is assembled in the USA and meets the Buy America(n) government procurement requirements under FAR, DFARS, and DOT regulations. [Click here for additional information.](#)

Catalog Logic



A810 LED

Weight: 1.0 lb.

Catalog Number	1	2	3	4	5	6	7	8
	A810							

1	LIGHT SOURCE & WATTAGES
	AM10W (10W ANP Module COB)
	AM20W (20W ANP Module COB)
	AM30W (30W ANP Module COB)

2	DISTRIBUTION
	D (Dome Frosted Acrylic Wide Distribution Lens)

3	CCT (COLOR TEMPERATURE)
	27K (2700K)
VERIFY	30K (3000K)
	35K (3500K)
	40K (4000K)
	50K (5000K) Consult Factory

4	DRIVER HOUSINGS
VERIFY	Wall Mount Options Choose Arm in Box 5
	RTCW (Driver Housing for Wall only (E-arms); 6 1/2" OD x 5 5/8" H)
	SRTCW (Smooth Driver Housing for Wall only (E-arms); 6" OD x 2 7/8" H)
	RTCNCW (Driver Housing/No Spun Cover for Wall only (E-arms); 5 7/8" OD x 2 5/8" H)
	Stem Mount Options Choose Stem Size in Box 5
	ST-RTCC (Driver Housing for Stem only; 6 1/2" OD x 5 5/8" H)
	ST-SRTCC (Smooth Driver Housing for Stem only; 6" OD x 2 7/8" H)
	ST-RTCNC (Driver Housing/No Spun Cover for Stem only; 5 7/8" OD x 2 5/8" H)
	Emergency Backup Housing Options Choose EMG Driver in Box 7
	ST-DCEM (Emergency Ballast Housing for Stem only; 12" OD x 1 1/2" H)
	*Driver Housing finish will match fixture finish.

5	MOUNTING SOURCES*
	Arm Mounts
	E3 E4 E6 E7 E8 E10 E11 E12 E15 E18 E25 E36
	Wall Mounts
	WM54 WM317
	Stem Mounts
	1/2" (13/16" OD) Rigid Stems 3/4" (1" OD) Rigid Stems
	2ST6 2ST12 2ST18 3ST6 3ST12 3ST18
	2ST24 2ST36 2ST48 3ST24 3ST36 3ST48
	2ST60 2ST72 2ST96 3ST60 3ST72 3ST96
	*Arm mount, Wall mount or Stem finish will match fixture finish.

6	GLASS & GUARDS
	100GLCL (Clear Glass)
	100GLFR (Frosted Glass)
	100GLPR (Prismatic Glass)
	100GLCLGUP (Clear Glass & Small Wire Guard)
	100GLFRGUP (Frosted Glass & Small Wire Guard)
	100GLPRGUP (Prismatic Glass & Small Wire Guard)
	100GLCLGUPC (Clear Glass and Cast Guard)
	100GLFRGUPC (Frosted Glass and Cast Guard)
	100GLPRGUPC (Prismatic Glass and Cast Guard)
	100GLGUP (Small Wire Guard with No Glass)
	100GLGUPC (Small Cast Guard with No Glass)
	*Cast and Wire Guard finish will match fixture.

7	MOUNTING ACCESSORIES
	EMG-LEDR05 (5W, LED Emergency Driver, Remote Placement up to 30', ANP Module Only, Provided with Flexible Conduit)
	EMG-LEDR7.8 (7.8W, LED Emergency Driver, Remote Placement up to 30', ANP Module Only, Provided with Flexible Conduit)
	EMG-LEDR10.7 (10.7W, LED Emergency Driver, Remote Placement up to 30', ANP Module Only, Provided with Flexible Conduit)
	EMG-LEDR17 (17W, LED Emergency Driver, Remote Placement up to 30', ANP Module Only, Provided with Flexible Conduit)
	GR07 (7" Wire Grill)**
	PC (Button Photo Cell) Remote Only
	SC (Scroll for Arms)**
	SLC (Sloped Ceiling Mount Canopy, 20" Max)**
	SQ (Square Back Plate)**
	SWL (Adjustable Locking Swivel)**
	TBK (Turn Buckle Kit)**
	*Most EM Packs are remote mount only (consult factory). For percent lumens in emergency mode, see Technical Section at www.ANPlighting.com
	**Accessory finish will match fixture finish.



Job Name:
Langus Mixed Use

Catalog Number:
A810-AM10W-D-**K-E25

Notes:

Type:
OW3

ELL25-143233



Specifications A810

Project Name: _____

Fixture Type: _____ Quantity: _____

Customer: _____

VERIFY

8 FINISHES					
*Premium and Marine Grade Finish have additional charges					
Standard Colors	Standard Grade	Marine Grade*	Premium Colors	Premium Grade*	Marine Grade*
Aspen Green	10	10M	High Gloss Black	01	01M
Cantaloupe	11	11M	Arctic Silver	14	14M
Lilac	12	12M	Candy Apple Red	64	64M
Putty	13	13M	Cobalt Blue	65	65M
Weathered Copper	29	29M	Caramel	66	66M
Black	41	41M	Butterscotch	67	67M
Forest Green	42	42M	Black Silver	68	68M
Bright Red	43	43M	Gunmetal Gray	69	69M
White	44	44M	Mayan Gold	79	NA
Bright Blue	45	45M	Textured Desert Stone	80	80M
Sunny Yellow	46	46M	Extreme Chrome	81	81M
Aqua Green	47	47M	Graystone	82	82M
Galvanized	49	NA	Oil Rubbed Bronze	83	83M
Navy	50	50M	Carbon Graphite	96	96M
Architectural Bronze	51	51M			

Consult Factory for additional paint charges and availability.

8 FINISHES					
*Premium and Marine Grade Finish have additional charges					
Standard Colors	Standard Grade	Marine Grade*	Premium Colors	Premium Grade*	Marine Grade*
Patina Verde	52	52M			
Copper Clay	53	53M			
Silver	56	56M			
Black Verde	61	61M			
Painted Chrome	70	70M			
Painted Copper	71	71M			
Textured Black	72	72M			
Matte Black	73	73M			
Textured Architectural Bronze	76	76M			
Textured White	77	77M			
Textured Silver	78	78M			

Consult Factory for additional paint charges and availability.

PERFORMANCE TABLE - ANP MODULE - INITIAL LUMENS																						
LED CATALOG	WATTS	DISTRIBUTION	2700K				3000K				3500K				4000K							
			INITIAL LUMENS	lm/W	B	U	G	INITIAL LUMENS	lm/W	B	U	G	INITIAL LUMENS	lm/W	B	U	G					
AM10W	10W	TYPE V	847	85	1	0	0	888	89	1	0	0	899	90	1	0	0	910	91	1	0	0
AM20W	20W	TYPE V	1694	85	1	0	0	1777	89	1	0	0	1799	90	1	0	0	1820	91	1	0	0
AM30W	30W	TYPE V	2541	85	1	0	0	2665	89	1	0	0	2698	90	1	0	0	2730	91	1	0	0

MODULE SPECIFICATION
• CREE XLAMP™ CMU
• Typical Efficacy 90 lm/W
• CCT: 2700K, 3000K, 3500K, 4000K
• CRI: 90 (min) 92 Typica
• 3-Step Ellipse
• Certifications: UL Recognized, RoHS, ENEC, REACH

DRIVER SPECIFICATION
• Universal Input: 120-277V
• Dimming: 0-10V, TRIAC, ELV (Dims to 1%)
• Power Factor @ Full Load >90%
• THD @ Full Load <20%
• Constant Current Output 50/60Hz
• Operating Temperature: -30°C Minimum
• Lifetime: >50,000 hours
• Ingress Protection: IP66
• Certifications: UL Recognized, CE, FCC, Energy Star, DLC, CA Title 24

FINISHES
• A polyester powder coat high quality finish is electro-statically applied and baked at 430° for exceptional durability and color retention. Products undergo an intensive five-step cleansing and pretreatment process for maximum paint adhesion.
• Marine grade finish provides superior salt, humidity and UV protection. This coating withstands up to 3,000 hours of continuous salt spray, comes with a 5-year warranty and is available in either a textured or gloss surface.

WARRANTY
See www.ANPlighting.com for complete fixture warranty.
LED warranty information
• 5 year limited warranty*
• *Limited Warranty: A typical year is defined as 4,380 hours of operation.





Specifications A810

Project Name: _____

Fixture Type: _____ Quantity: _____

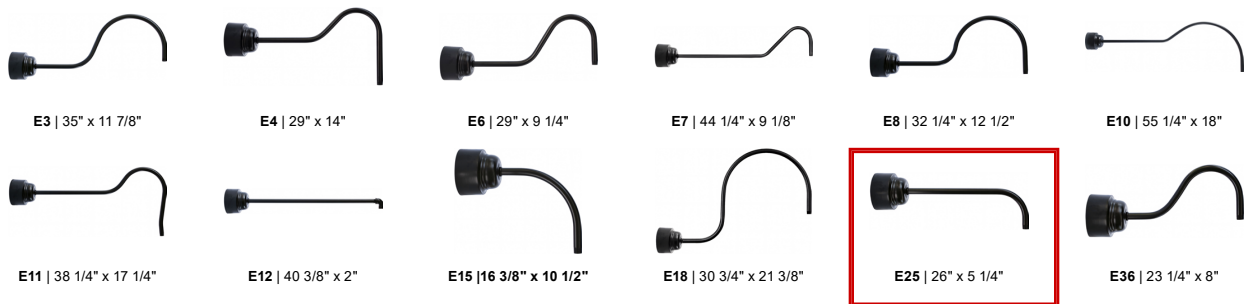
Customer: _____

ACCESSORIES



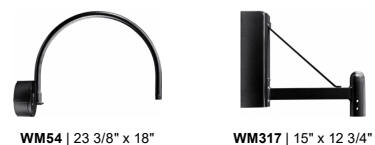
POPULAR ARM MOUNTS

Dimensions are Projection x Height | CB included with all arms [Click here for all Arm Mount styles](#)



POPULAR WALL MOUNTS

Dimensions are Projection x Height [Click here for all Wall Mount styles](#)



DRIVER HOUSINGS





Job Name:
Langus Mixed Use

Catalog Number:
A810-AM10W-D-**K-E25

Notes:

Type:
OW3

ELL25-143233



Specifications A810

Project Name: _____

Fixture Type: _____ Quantity: _____

Customer: _____

GLASS GUARD

▶ CL = Clear ▶ FR = Frosted ▶ PR = Prismatic ▶ 100 = Small ▶ 200 = Large



Glass Only
6 1/2" H X 4 1/8" W
100GLCL
100GLFR
100GLPR



Glass with Cast Guard
7 1/2" H X 4 1/8" W
100GLCLGUPC
100GLFRGUPC
100GLPRGUPC



Glass with Wire Guard
7 1/2" H X 4 1/8" W
100GLCLGUP
100GLFRGUP
100GLPRGUP



GUPC Guard Only
7 1/2" H X 4 1/8" W
100GLGUPC (Cast Guard)



GUP Guard Only
7 1/2" H X 4 1/8" W
100GLGUP (Wire Guard)

FINISHES COLORS

STANDARD



PREMIUM



Key Observations:

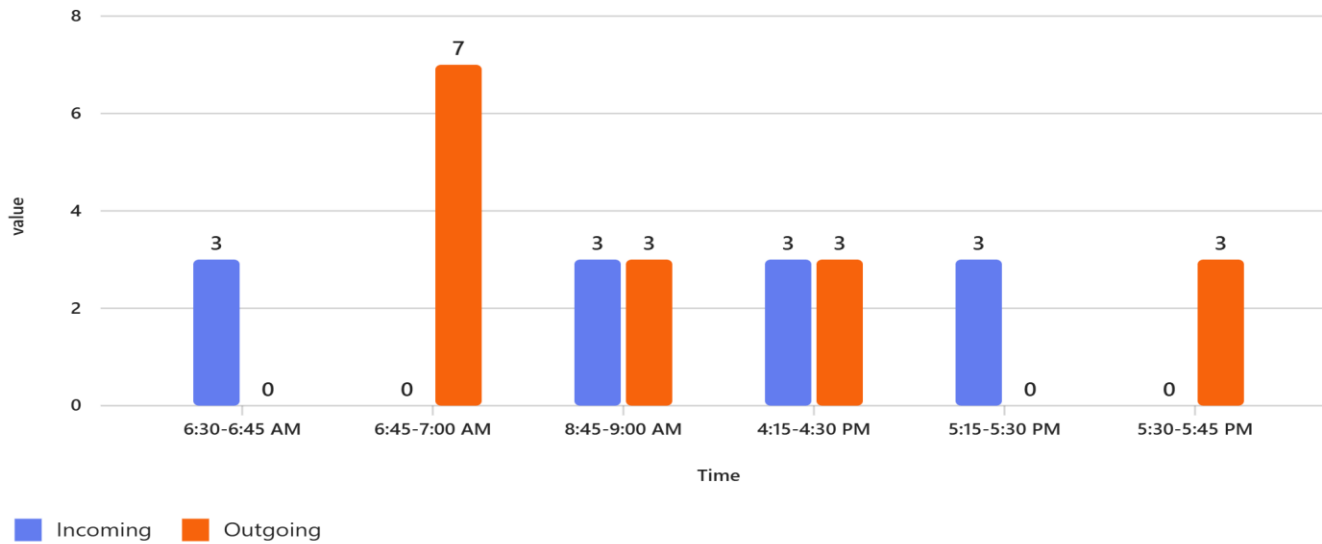
Hoff Apartments (48 units): Morning peaks are modest, with the highest outgoing traffic at 6:45–7:00 AM at **.15 trips / unit (7 vehicles)**.

: Afternoon traffic is balanced, peaking around 4:15–4:30 PM and 5:30–5:45 PM at **.06 trips / unit (3 vehicles)**

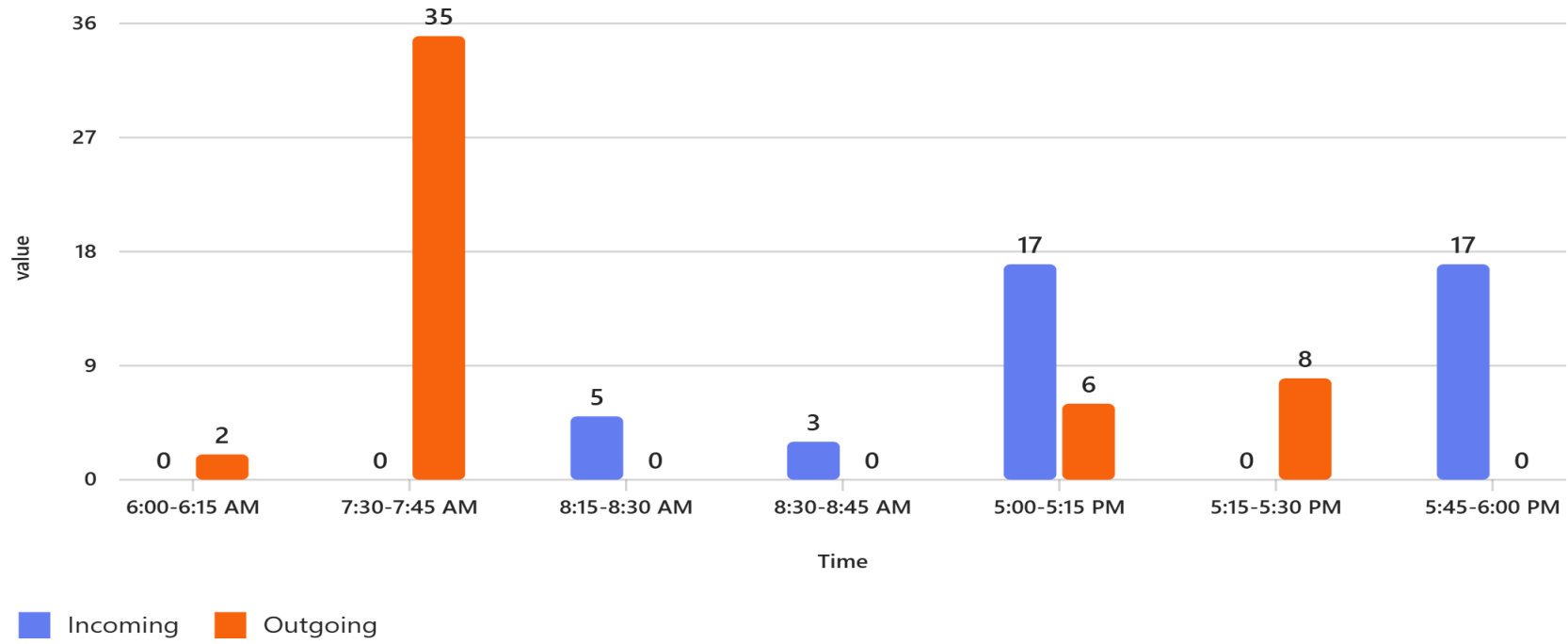
LeSilve Apartments (180 units): Much higher volumes overall due to the project size . Morning peak outgoing traffic is at 7:30–7:45 AM at **.19 trips / unit (35 vehicles)**.

:Afternoon peak incoming traffic occurs at 5:00–5:15 PM and 5:45–6:00 PM at **.09 trips / unit (17 vehicles each)**.

HOFF



LeSilve 180 apts





Village of Mount Horeb

Economic Impact of New Mixed-use Development

Proposed development at 214-220 E. Main St.

Kristen Fish-Peterson – Redevelopment Resources
11-24-2025

Economic Impact Analysis for Proposed Development at 214 -220 E. Main St.

Introduction

Upon review of the potential new development at 214-220 E. Main St. in Mount Horeb, there are a few foundational items to understand about downtown, infill redevelopment and greenfield development (starting from scratch on a clean site) elsewhere in the community. The differences between infill development and greenfield development are significant.

Advantages (for the developer) to greenfield development include:

- Easier, faster construction with open space and flexible layout.
- Lower land acquisition and preparation costs.
- Ample room for parking and expansion.

Disadvantages to greenfield development include:

- Requires new infrastructure (roads, water, sewer, stormwater).
- Increases long-term municipal service and maintenance costs.
- Generates car-dependent development, reducing downtown activity.
- Contributes to sprawl and loss of farmland or open space.

If a similar development was to be constructed in a downtown area, on a redevelopment, or infill site, here are the advantages and disadvantages:

Advantages to redevelopment/infill development:

- Leverages existing infrastructure (streets, utilities, sidewalks).
- Strengthens downtown vitality and walkability.
- Supports local businesses through added foot traffic.
- Aligns with smart growth and infill development goals.

Disadvantages to redevelopment/infill development:

- Higher site preparation costs (demolition, remediation, tight access)
- Complex permitting or design review due to historic context
- Limited space for staging and parking
- Potential for neighboring disruption during construction.

For the above reasons, the case can be made that infill development serves communities like Mount Horeb well, in that it is an efficient use of existing infrastructure and will strengthen downtown vitality and walkability, decreasing sprawl and dependence on vehicles. The very specific economic and fiscal impacts of the proposed development at 214-220 E. Main Street are discussed in the remainder of this report.

Economic Impact

An economic impact analysis was conducted for the proposed development at 214-220 E. Main St. The analysis assumes the following:

- a minimum capital investment of \$18,000,000
- development of 44 residential units (8 studios, 26 1-bedroom units, 10 2-bedroom units)
- approximately 57 people will occupy the building in the living units.
- 9,000 square feet of commercial space.

Initially, the construction of a \$18 million development will have a significant impact on the local economy, as evidenced by the following table:

Economic Impact:					
Impact Type	Employment (Job Years)	Employee Compensation	Labor Income	Value Added	Gross Output
Direct Effect	106	\$5,528,149	\$7,886,764	\$8,281,822	\$18,686,864
Indirect Effect	39	\$2,442,256	\$2,729,637	\$4,376,955	\$8,603,028
Induced Effect	51	\$2,609,943	\$2,894,348	\$5,125,404	\$8,917,541
Total Effect	195	\$10,580,347	\$13,510,749	\$17,784,181	\$36,207,433
<small>Source: Associated General Contractors of America Construction Impact Model (https://www.agc.org/agc-construction-impact-model)</small>					
Sales (Total Output): Top 10 Industries Impacted					\$
1	Construction of new multifamily residential structures				\$19,399,938
2	Owner-occupied dwellings				\$1,289,921
3	Retail - Building material and garden equipment and supplies stores				\$997,448
4	Hospitals				\$856,585
5	Other real estate				\$541,064
6	Insurance carriers, except direct life				\$471,535
7	Offices of physicians				\$449,400
8	Ready-mix concrete manufacturing				\$378,570
9	Monetary authorities and depository credit intermediation				\$370,539
10	Retail - Nonstore retailers				\$366,484

Direct effects are the immediate changes, the construction of the mixed used development. For this project equals \$18,686,864.

Indirect effects are the ripple effects through supply chains, such as increased demand for raw materials. For this project, an additional \$8,603,028 impact is estimated.

Induced effects are the changes in household spending due to the new jobs created, and spending that occurs from wages paid to construction workers and employees of suppliers in the supply chain, and increased spending at local businesses. The table above indicates the total induced impact of the construction of the proposed building at 214-220 E. Main St. For this project equals an additional \$8,917,541.

Total economic impact just from construction on this project will equal approximately **\$36,207,433**.

Fiscal Impact (Revenue)

- Potential assessed value: \$10,500,000 (incremental value added to TID #5)
- Estimated annual property taxes paid by owner: **\$188,000/year, average** (over the remaining life of TID #5 that would equal \$2,832,473)
This assumes the property is constructed in 2027 and paying taxes 2028-2043 when the district closes.
- Utility revenue
- Current/existing development is assessed at \$714,700 total for all three parcels. These parcels generate \$11,428 annually in property taxes.

Economic Impact on the community from approximately 83 new mid-high-income earners living downtown:

Assuming residents earn between \$60,000-\$80,000 per year (spending 30% of their income on rent at market rates), and there will be 57 people living in this building, the economic impact of spending a portion of their disposable income downtown would be significant:

Living Expenses (not included in what would be spent downtown/disposable income):

Rent: \$1,500-\$2,000/month

Utilities (electricity, gas, internet, phone): ~\$300/month

Insurance (health, renters, auto): ~\$500/month

Vehicle costs (payment, gas, maintenance, insurance): ~\$800/month

This totals approximately:

Low end: \$3,100/month

High end: \$3,600/month

= \$37,200-\$43,200/year in core living expenses.

Remaining disposable income would be:

Annual Income	Basic Living Costs	Disposable Income
\$60,000	~\$37,200	~\$22,800
\$80,000	~\$43,200	~\$36,800

Average disposable income per person \approx \$30,000/year

Multiply by Number of New Residents

57 residents \times \$30,000 \approx \$1.71 million in total disposable income

Estimated Portion Spent Downtown

Let's assume a conservative 25–40% of that disposable income is spent in the downtown on: dining and coffee, retail and personal services, entertainment, professional services (banking, salons, fitness, etc.)

25% of \$1.71M = \$427,500/year

40% of \$1.71M = \$684,000/year

The downtown area could see an annual economic boost of **\$427,000 to \$680,000 per year** in new spending from the 57 new residents, based on average disposable income after fixed costs.

It's highly likely the income of the residents in the proposed building will be earning more than \$60,000–\$80,000/year.

This analysis does not include the additional positive economic impact from the approximate 90 residents who will reside at the Steve Brown Apartment building also proposed for downtown.



AGENDA ITEM REPORT

MEETING DATE

January 7, 2026

PREPARED BY

Nicholas Owen, Administrator

AGENDA ITEM # 6.b.1

Consideration of General Development Plan, Planned Development Rezone Application, Jeff Grundahl, Langhus on Main, 208-220 E Main Street

BACKGROUND

I have added some documents to the packet to address a complaint received from a resident regarding the developer not addressing the economic impact of the proposed development on the surrounding neighborhood. Included in the revised packet are the developer's response on this topic, including highlights to sections of a study conducted by the City of Sun Prairie Community Development Authority on the impact of multifamily housing on nearby property values. The new attachments also include comments from Village Planner Ben Rohr, who also references this study in his Plan Commission Memo on the General Development Plan from the November meeting.

I also have included in the new attachments an updated memo from Village Engineer Rob Wright on the traffic demand based on the reduction in residential units included in the recommended proposal.

RECOMMENDATION

ATTACHMENTS

1. MEM 2025-1126 Langhus Development GDP Revision
2. letter to Village board re complaint on property values
3. Impact of a New Mixed Use on Surrounding Property Values
4. Vandewalle response to complaint
5. Housing Values Study Report Highlights
6. MEM 2025-1126 Langhus Development GDP Revision

PROJECT Village of Mount Horeb – General Review DATE 11/26/2025

PROJECT NO. 00050581.001

SUBJECT Langhus GDP Review

PREPARED BY Robert Wright

NAME	COMPANY
Nic Owen – Village Administrator	Village of Mount Horeb
Ben Rohr – Village Planner	Vandewalle & Associates

NOTES

We have reviewed the information supplied by the Developer (JG Development) related to the CSM, Traffic Impact Analysis, and GDP for the 4-story mixed use Building at 210.214 and 220 E. Main Street. The developer’s proposal has been revised with removal of approximately 22 units for a total unit count of 44 (66 in original proposal). As we had previously discussed stormwater and utility connections and they have remained unaffected by the revised proposal, this review will focus on potential Traffic Impact

TRAFFIC IMPACT

A Traffic Impact Analysis (TIA) was submitted by the Developer 8/14/2025. The traffic counts were taken during summer (non-school day traffic), however they fall closely in line with past traffic counts completed by WISDOT and the MHPD. During public hearings related to the original project proposals, there were comments related to school vs. non-school session traffic analysis (Summer vs. Fall). Because the analysis was completed in the summer (non-school) along with the MHPD study, we will focus on the study as presented. Also, the original study assumed a full residential buildout of 66 units, while the revised proposal will be for 44 units. We should not assume that the entire 1/3 of the traffic to be reduced from what was presented originally, however this may be a close approximation for this review.

The site will be accessing off the alley parallel to Main Street and off N. 3rd Street. The Zoning Adjacent to 3rd is Zoned CB, and allows for direct access to this roadway.

Trip generation for the development follows the ITE and were limited to 220 total trips per day (110 in and 110 out) 30 of these trips were Peak hour AM, and 25 during Peak hour PM. This will be recued with the current proposal.

Current Level of Service (LOS) for the area for the 2nd/Main and 3rd/Main intersection were functioning at or above a LOS of D, the development does cause an additional decrease of LOS (LOS from D to E) in one scenario at 3rd/Main southbound from 3rd to Main, with an increase in delay time of about 7-seconds. Delay and queuing time are still within reasonable amounts. This should decrease in relation to the reduced unit counts and potentially provide a higher LOS based on reduced trip generation.

Review of the parking spots on the east side of 2nd and the west side of 3rd should be undertaken to adhere to Wisconsin State Statute 346: Rules of the Road for parking sizes; clearance to alley/drives; and clearance to stop/signs and crosswalks. This may result in removal of parking spots to follow Wis. Stats. 346.

As the pedestrian crossing and issue is important to the Village as a whole, some of the existing pedestrian issues are both standalone and potentially further impacted by the development. We continue to suggest additional pedestrian crossing improvements should be made to the Main Street crossings due to high pedestrian usage. We have been in discussions with MHPD and Public Services related to signage and marking in response to current residents' concerns.

Complaint regarding surrounding property values

Dear village board members.

I am writing to address the concern and complaint that was filed by Michelle Kelley on 12/22/25 citing section 6b of the ordinance regarding "**Economic feasibility and impact**". The petitioner shall provide evidence satisfactory to the Village Board that the proposal will not adversely affect the values of surrounding properties."

In response, I would offer the attached documents in response to this complaint. I would like it to note, however, that according to Nic Owen/Village administrator, there is no precedence that he could uncover where the village board has required this of any other developer/development in past project approvals including the most recent Steve Brown and JT Klein project approvals. I am responding to this as a courtesy to Ms Kelley and the residents.

Following are independent studies, sources and specific responses directly related to the nature of the complaint.

Please see the following:

1. Letter and direct links to sources regarding value
2. Letter from Village planner Ben Rohr
3. Housing Values Study report/ FINAL from the city of Sun Prairie WI-lengthy document, **please see highlighted summary text on** page 1-3.

I trust that these independent studies will show that the evidence suggests that there is no negative effect in any area of the state, county or world (in some studies cited global information) that suggest that market rate/mixed use development will negatively affect neighboring properties.

Sincerely,

Jeff Grundahl

Impact of a New Mixed-Use Development on Surrounding Residential Properties prepared

Subject property

-44 market rate apartments

-9,000 sq ft of retail space

General Evidence on Property Value Changes

Positive or Neutral Impacts Predominate: Numerous analyses of mixed-use developments (combining residential and commercial elements like retail) indicate they tend to boost nearby home values by improving neighborhood desirability. For instance, one pricing study found that households are willing to pay up to 6% more for homes in mixed-use neighborhoods compared to similar ones in single-use (for example, purely residential) areas, provided the mix avoids heavy industrial elements that could introduce noise or pollution. This premium stems from benefits like convenient access to shops and services, which outweigh potential drawbacks like minor traffic increases in well-designed projects.

No Evidence of Widespread Decreases: A comprehensive review of U.S. studies on housing density and mixed-use projects found no instances where gentle density (e.g., mid-rise apartments with ground-floor retail) harmed single-family or rental property values. Instead, proximity to such developments often correlates with higher price appreciation. A 2005 Urban Land Institute report echoed this, noting that well-integrated higher-density mixed-use projects either have no impact or slightly positive effects on nearby single-family home values, with every reviewed study concluding an overall increase in neighborhood values.

Specific Studies and Examples

Here's a summary of key research, highlighting contexts similar to a small-scale mixed-use project:

Study/Source	Location & Context	Key Findings on Property Values
Miami-Dade County Suburban Mixed-Use Districts (2014)	Florida suburbs; analyzed 7 districts with retail-residential mixes at varying proximities (¼–1 mile)	Net positive impact on adjacent values, explaining 10–71% of price variance in some cases (e.g., +\$0.072/sq ft per foot closer in Dadeland district). Stronger benefits in pedestrian-friendly, diverse areas; multi-family homes saw bigger gains (up to 20%+ variance explained) than single-family. core.ac.uk
Salt Lake County Apartment Proximity Study (2010–2018)	Utah suburbs; focused on single-family homes near new apartments	Homes within ½ mile of new apartments appreciated faster than those farther away, attributing gains to added amenities and vitality without significant negatives. masslandlords.net
Montgomery County Mixed Land Use (2017 Student Research)	Maryland; effects of new mixed-use on nearby homes	New developments increased values for homes outside the project but nearby, though some noise/traffic could be perceived negatively in specific cases (e.g., near universities); overall positive spillover. scholar.umw.edu
Mixed-Income Multi-Family Housing Impacts (MIT Study)	Massachusetts; large, dense projects (expected to have strongest negatives)	No negative impacts on surrounding values, even for sizable developments; counters assumptions about density harming desirability.
Rotterdam Hedonic Pricing Analysis (2010)	Netherlands urban region; examined mixed land use within 500m buffers around homes	One standard deviation increase in land-use diversity raised home prices by 1.4%–3.9% (€2,189–€6,694). Net positive effect of up to 6% for balanced mixes (e.g., retail + residential), but negative for incompatible uses like manufacturing (-2.6% in port areas). econstor.eu

Factors Influencing Outcomes

Scale and Design Matter: Small projects like this one (44 units + retail) are less likely to cause issues like overwhelming traffic or congestion compared to large-scale ones. Studies emphasize that positive effects are amplified in well-planned developments with pedestrian access, and compatible retail (shops vs. heavy industry). Poor design or mismatched uses could lead to minor negatives, but evidence shows these are rare for market-rate mixed-use.

Broader Economic Benefits: Such developments can stimulate local economies by attracting residents and shoppers, increasing demand for nearby housing. A 2012 study on walkable neighborhoods linked mixed-use density to value increases for both single-family and multi-family properties due to better access to retail and parks.

Addressing the Concern of Decreases: Searches for negative evidence yielded limited results, often tied to unrelated factors like affordable housing stigma (not applicable here) or industrial mixes. One review noted a "lack of consensus" on commercial spillovers causing negatives, but empirical data leans positive.

No U.S. studies specifically on market-rate mixed-use found consistent decreases.

In summary, the evidence suggests this development would likely increase or at least not decrease surrounding residential property values, countering the expressed fear.

<https://masslandlords.net/gentle-density-increases-nearby-property-values-evidence-shows-contrary-to-popular-belief/>

<https://onlinelibrary.wiley.com/doi/abs/10.1111/j.1467-9787.2012.00776.x>

<https://ideas.repec.org/p/tin/wpaper/20100105.html>

https://www.researchgate.net/publication/228181883_The_Impact_of_Mix_Land_Use_on_Residential_Property_Values

Re: Complaint to file

From Benjamin Rohr <brohr@vandewalle.com>
Date Mon 1/5/2026 9:23 AM
To Nic Owen <Nic.Owen@mounthorebwi.info>

Nic,

My staff report referenced the Redevelopment Resources Economic Impact Analysis on the project as part of the evidence that the development would not have adverse impacts economically. That analysis was more focused on economic impacts at the Village-level than the surrounding properties. Nothing specifically was discussed about economic impact on surrounding properties as part of the Plan Commission meetings.

I dislike this type of language within ordinances because it is difficult to understand in real time what potential long-term economic impacts will be on surrounding properties. All we can do is rely on other situations that have occurred elsewhere as potential evidence. However, we know that this is a unique development situation that probably doesn't have a perfect 1-to-1 comparison to analyze. For this reason, no such language exists within the draft new code.

Further, there is an issue related to what the ordinance means by "surrounding". Is that a property within 50 feet, 100 feet, 500 feet, 1,000 feet, etc. of the project? Someone could reasonably assume that it includes those properties that share a property line with the project, but to what extent beyond that does the area of impact include? That is not defined. As such, the impact of the project on surrounding properties could have different impacts depending on the scale you choose to analyze.

We had a similar discussion with the 1881 Springdale project in 2024 where the neighbors to the south of the project brought up this same ordinance reference. At that time, we referenced [Sun Prairie's 2024 Multi-Family Housing Impact on Nearby Property Values Analysis](#). This report shows appreciating values in single-family neighborhoods adjacent to multi-family development. The Appendix also includes other research on the subject from around the country that confirms the same trends.

Overall, every community, project, situation, circumstance, and surroundings are slightly different and unique. My assumption would be that property values for single-family homes in the neighborhoods surrounding downtown have appreciated in value over the past 10 years. In that time, there have been new businesses, buildings, and developments that have occurred downtown, which did not have adverse economic impacts on surrounding neighborhoods. However, obviously nothing like the proposed project can be referenced as a 1-to-1 comparison from the recent past in Mount Horeb.

I hope this helps. Regarding Wednesday's meeting, is it your expectation that I attend and participate in the meeting or do you have it covered? I know we talked about it some in December, but haven't talked about it since then.

Thanks.

Ben Rohr, AICP

Vandewalle & Associates Inc.

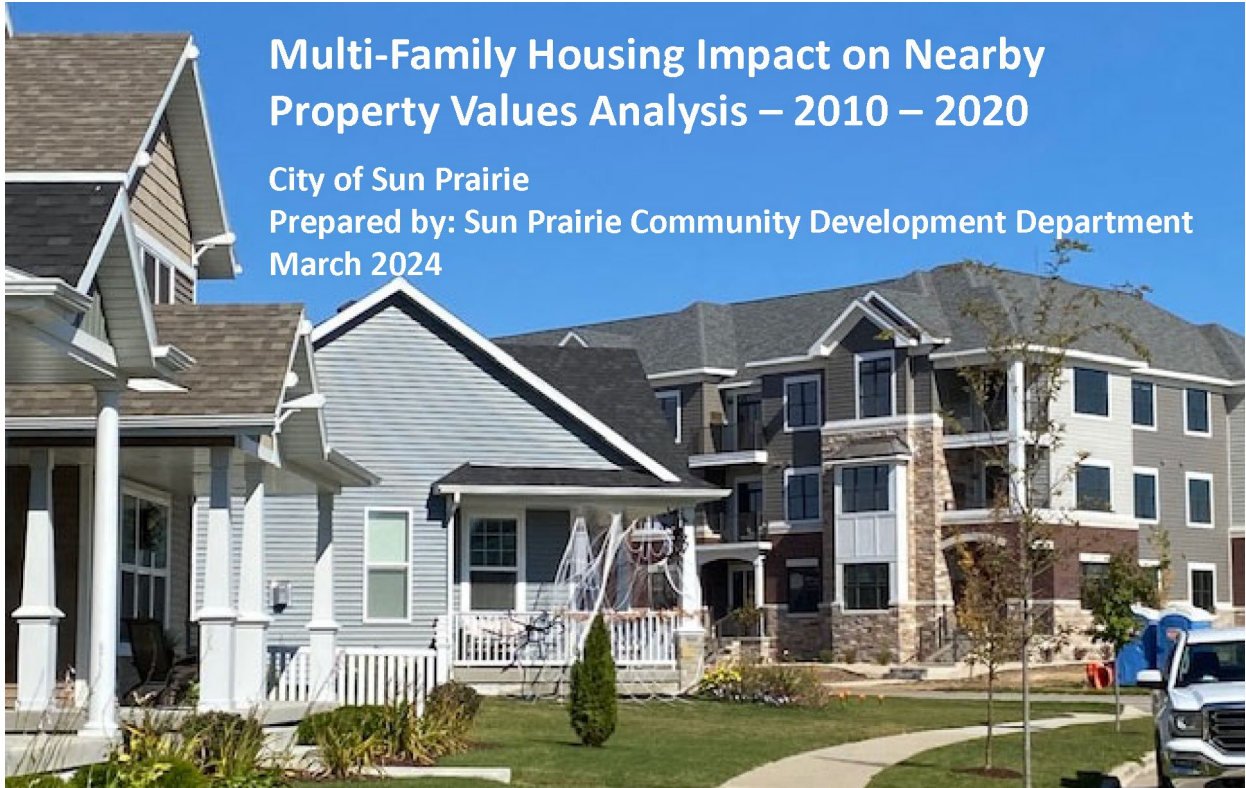
Shaping places, shaping change

120 E. Lakeside St.

Madison, WI 53715

CELL PHONE: 608-449-4912

www.vandewalle.com



Multi-Family Housing Impact on Nearby Property Values Analysis – 2010 – 2020

City of Sun Prairie

Prepared by: Sun Prairie Community Development Department

March 2024

Background

In an effort to meet high demand in a regional and local housing market that is experiencing strong growth and a scarcity of housing supply, developers continue to promote development of new market rate and affordable multi-family units within the City of Sun Prairie and surrounding communities. The City's strategic planning goals established by the City Council prioritize efforts and actions that help to address housing supply and affordability, as well. However, such proposals are often met with opposition from neighboring property owners, neighborhood associations, and, on occasion, other developers with development interests in surrounding properties. While there are typically a variety of concerns raised at public hearings and neighborhood meetings on such projects, there is a predominant fear among many regarding the potential negative impacts of multi-family development on neighboring property values.

Although there is little evidence to support the notion that market rate and affordable multi-family development has a negative effect on nearby single-family property values, this continues to be a strong narrative at Plan Commission and City Council meetings from residents opposed to such development projects in their neighborhoods. Across the Nation, numerous studies have been conducted to try to substantiate this claim and measure such impacts, but largely such studies have found that there is no relationship between single-family property values and proximity to multi-family or affordable housing. However, it should be noted that any type of poorly managed or designed development, whether residential, commercial, or otherwise, can have a negative impact on property values and neighborhood perception. This emphasizes the importance of good planning and design, and building maintenance/management. Please see Appendix A for a summary of studies and general findings of some of these national efforts.

City of Sun Prairie staff has found that sharing the results of such national studies has provided little comfort to concerned residents. Unless corroborated locally, many feel that such studies are not applicable to the Sun Prairie housing market. To many, it is commonly accepted that a negative relationship exists between single-family homes in close proximity to multi-family development and neighboring property values, and it is assumed to be true for many despite there being little, if any, data to support this position.

This analysis is not intended to duplicate the prior studies on this topic noted above within Sun Prairie. Limited staff time and resources do not allow the completion of an extensive study at this time. Instead, this is an effort to look at actual single-family home sales data within Sun Prairie to determine whether there is any reason to suspect that our local experience is any different from the conclusions reached by the research studies that have been completed nationwide on the topic, and whether more detailed analysis locally is warranted.

Summary

This analysis sought to study actual sales of single-family homes within the City of Sun Prairie between 2010 and 2020 to determine whether homes located close to multi-family housing appreciated in value at a different rate than those located farther away from multi-family properties. Homes that sold multiple times over the course of the decade were selected, and rates of appreciation compared between properties in the following categories: 1) those properties located less than 300 feet from multi-family development; 2) those located between 300 and 1,000 feet of multi-family development; and 3) those located more than 1,000 feet away from multi-family sites. The findings of this analysis *do not* support the notion that multi-family development has a negative impact on surrounding property values. In fact, a higher percentage of homes located within 300 feet of multi-family sites saw annual appreciation rates of over 4% than those located farther away, as illustrated in the following table:

Annual Appreciation Breakdown							
Annual Appreciation	<300'		300' - 1000'		>1000'		
	#	%	#	%	#	%	
0-2%	97	39%	251	40%	308	45%	
2-4%	68	28%	207	33%	240	35%	
4-6%	41	17%	93	15%	79	12%	
>6%	41	17%	79	13%	59	9%	
Total # records	247		630		686		

Of the 247 homes located within 300 feet of multi-family housing that sold multiple times during the decade, 34% of them appreciated at an annual rate of 4% or higher. Although the number of properties with multiple sales in the other geographies were greater, only 28% of homes located within 300 to 1,000 feet and 21% of homes located beyond 1,000 feet from multi-family sites appreciated at this rate.

Staff does not believe that there is enough data and evidence here to support the notion of a positive causal relationship between property values and proximity to multi-family development. However, the

results do strongly support the applicability of the findings found nationally to our local Sun Prairie housing market. It does not appear that the values of single-family homes are negatively affected when located near multi-family housing.

Methodology

The following outline describes in more detail the steps involved in the analysis of this issue using local home sales data available to the Assessing Department. The analysis was a combined effort by the City's Community Development, Assessing, GIS and IT staffs.

Data Collection

- Staff obtained a multi-family point file from the Madison Metropolitan Planning Organization (MPO), with MF locations from 2016. The data was verified and updated by staff to establish the multi-family districts upon which the study was based.
- Staff obtained Market Drive assessing data from City Assessor, which included data on all property sales that occurred in the City between 2010 and 2020, as well as an evaluation of sales as to whether they were valid, arms-length transactions.
- The current City Parcel file from early 2020 was used for the analysis.

Data Processing and Screening

- Market Drive data was modified in the following manner:
 - The database was first modified to remove all but single-family parcels.
 - Further modification included the removal of all parcels that did not experience multiple sales over the course of the decade from 2010 to 2020.
 - The database was screened further to remove transactions deemed to not be arms-length transactions per Market Drive.
- The multi-family point file from the MPO was used to select and identify parcels with multifamily buildings.
- Once the multi-family parcels were established, three distinct cohorts were established using GIS based on distance from the multifamily parcels. Cohorts established were 1) all properties within 300 ft. of multi-family parcels, 2) those located between 300 ft. and 1,000 ft. from multi-family parcels, and 3) all other parcels located beyond 1,000 ft. from multi-family parcels.
- The selected single family Market Drive parcels were analyzed to derive an average annual appreciation using the formula " $100 \times ((\text{lastPrice}/\text{firstPrice})^{(1/\text{years})} - 1)$ ". This formula was derived from: <https://www.aqua-calc.com/calculate/home-value-appreciation>
- The Market Drive database was joined with the Sun Prairie parcel database to assign each parcel an average appreciation by year.
- Obvious outliers that appeared to be foreclosure 'flips' with wide disparity in sales prices were investigated and removed from the analysis where warranted. Some parcels were discovered to be 'parent' parcels that included a prior sale of a much larger original development parcel for a larger subdivision. In both cases, these sales had wide disparities in sales prices, and did not represent comparable sales of homes over time. These parcels were removed from the study.

Data Analysis and Presentation

- Properties within each cohort were sorted into bins for comparison, as follows: properties that appreciated between 0-2% annually, those appreciating between 2-4% annually, those appreciating between 4-6% annually, and those appreciating at a rate of more than 6% annually.

- Results were mapped to view the location of higher and lower appreciating properties visually, to make sure there did not seem to be any location factors affecting the data outside of proximity to multi-family parcels. Maps for each cohort are provided in Appendix B.
- The number of results in each bin were counted and percentages in each bin derived and reported in the following table:

Annual Appreciation Breakdown						
Annual Appreciation	<300'		300' - 1000'		>1000'	
	#	%	#	%	#	%
0-2%	97	39%	251	40%	308	45%
2-4%	68	28%	207	33%	240	35%
4-6%	41	17%	93	15%	79	12%
>6%	41	17%	79	13%	59	9%
Total # records	247		630		686	

Conclusions

The results of the analysis suggest that the findings of numerous nationwide studies are applicable to Sun Prairie: the values of single-family homes do not appear to be negatively affected when located near market rate or affordable multi-family housing. As with any type of use, residential or otherwise, poorly managed and/or designed developments can have a negative impact on surrounding property values and neighborhood perception. To this end, proactive planning, appropriate design, continued investment, and good property management are critical factors in maintaining/increasing property values.

Appendix A – Summary of Pertinent Research Studies

- A. [Effects of Multi-Family Housing on Property Values, Crime and Code Violations in Little Rock, 2000 – 2016](#). UALR Center for Public Collaboration. January 19, 2017

Summary: “The results indicate that: 1) Subsidized multi-family housing has a positive effect on the sales prices of single-family within 1000 feet and reduces the vulnerability of properties within 2000 feet to property crime. 2) Most forms of non-subsidized market-rate housing, including condominiums, market-rate apartments, and senior and assisted-living facilities, have either no effect or a positive effect on the sales prices of single family homes within 2000 feet. 3) Small (fewer than 5 buildings) market-rate apartment complexes, subsidized apartment complexes, and dormitories have either no effect on the vulnerability of properties within 2000 feet or they reduce crime vulnerability. 4) Large (five or more) market-rate apartment complexes and condominiums appear to increase the vulnerability of properties within 1000 feet to violent crime. The causal mechanism for this finding remains unclear. 5) Senior and assisted living apartments appear to increase the vulnerability of properties within 1000 feet to property crime. 6) Insufficient evidence exists at this time to determine the effect of multi-family housing on the vulnerability of nearby properties to code violations.”

- B. [The Impact of Multifamily Development on Single Family Home Prices in the Greater Boston Area](#). Master’s Thesis Arah Schuur, MIT. May 19, 2005

Summary: “Using data on the sales of single-family homes from 1987 until 2005, regression analyses were used to construct hedonic price models for the impact and control areas. This model was used to create a sales price index over time. The trend in the index of the impact zone and the control area was compared in the years immediately preceding the permitting of the multifamily development and the years following completion of the development in order to determine if the multifamily development affected sales prices in the impact zone. In the four cases for which there was appropriate data, no negative effects in the impact zone were found.”

- C. [Assessing the Impact of Affordable Housing on Nearby Property Values in Alexandria, Virginia](#). Christina Plerhoples Stacy and Christopher Davis, Metropolitan Housing and Communities Policy Center at the Urban Institute. April 22, 2022

Summary: “We find that affordable units in the city of Alexandria are associated with a small but statistically significant *increase* in property values of 0.09 percent within 1/16 of a mile of a development, on average—a distance comparable to a typical urban block. ... These findings should ease residents’ concerns about the impact of affordable housing on neighborhoods and bolster support for increased development.”

- D. [The Impact of High-Density Apartments on Surrounding Single-Family Home Values in Suburban Salt Lake County](#). Kem C. Gardner Policy Institute at the University of Utah. February 2021.

Summary: “This study found apartments built between 2010 and 2018 have not reduced single-family home values in suburban Salt Lake County. In response to accelerating housing prices over the last decade, the market continues to shift to denser development to slow this trend. However, denser development continues to be a politically controversial topic on city council agendas as existing residents often bring up negative impacts on home values. Single-family homes located within 1/2 mile of a newly constructed apartment building experienced higher overall price appreciation than those homes farther away.”

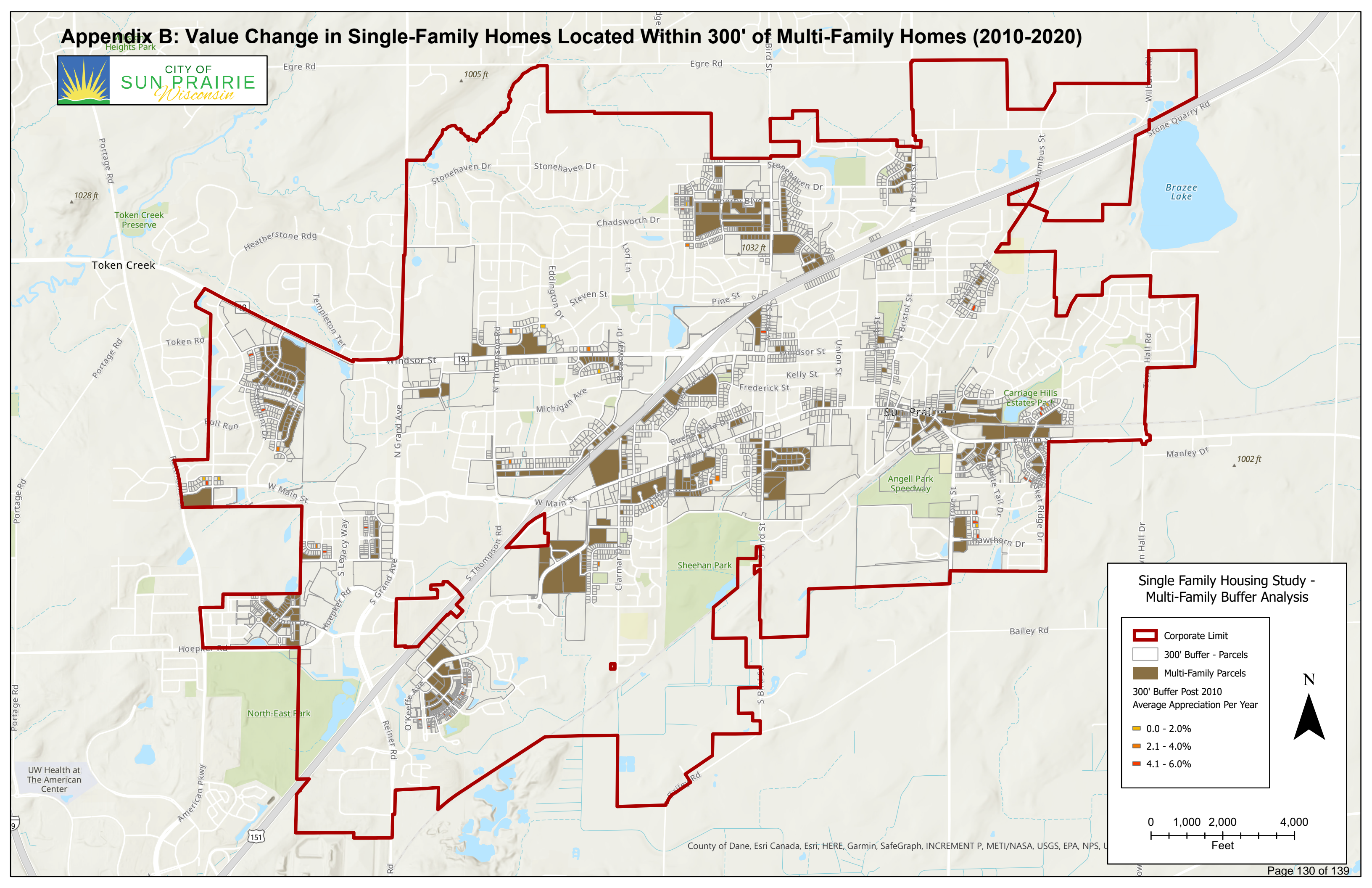
- E. [Does Federally Subsidized Rental Housing Depress Neighborhood Property Values?](#) Journal of Policy Analysis and Management, Vol. 26, No2 (Spring, 2007).

Summary: “Few communities welcome federally subsidized rental housing, with one of the most commonly voiced fears being reductions in property values. Yet there is little empirical evidence that subsidized housing depresses neighborhood property values. This paper estimates and compares the neighborhood impacts of a broad range of federally subsidized rental housing programs, using rich data for New York City and a difference-in-difference specification of a hedonic regression model. We find that federally subsidized developments have not typically led to reductions in property values and have, in fact, led to increases in some cases. Impacts are highly sensitive to scale, though patterns vary across programs.”

- F. [Low Income Housing Tax Credit Housing Developments and Property Values.](#) The Center for Urban Land Economics Research – The University of Wisconsin. June 2002.

Summary: “Our results for Wisconsin are generally consistent with results in other studies: we have not been able to find evidence that Section 42 developments cause property values to deteriorate. The exception is Milwaukee County, where properties that are distant from the developments seem to appreciate more rapidly, although the magnitude of the effect is small. We have found no evidence of an impact in Waukesha and Ozaukee, and find evidence that properties in Madison near Section 42 developments appreciate more rapidly.”

Appendix B: Value Change in Single-Family Homes Located Within 300' of Multi-Family Homes (2010-2020)



Single Family Housing Study - Multi-Family Buffer Analysis

- Corporate Limit
- 300' Buffer - Parcels
- Multi-Family Parcels

300' Buffer Post 2010 Average Appreciation Per Year

- 0.0 - 2.0%
- 2.1 - 4.0%
- 4.1 - 6.0%

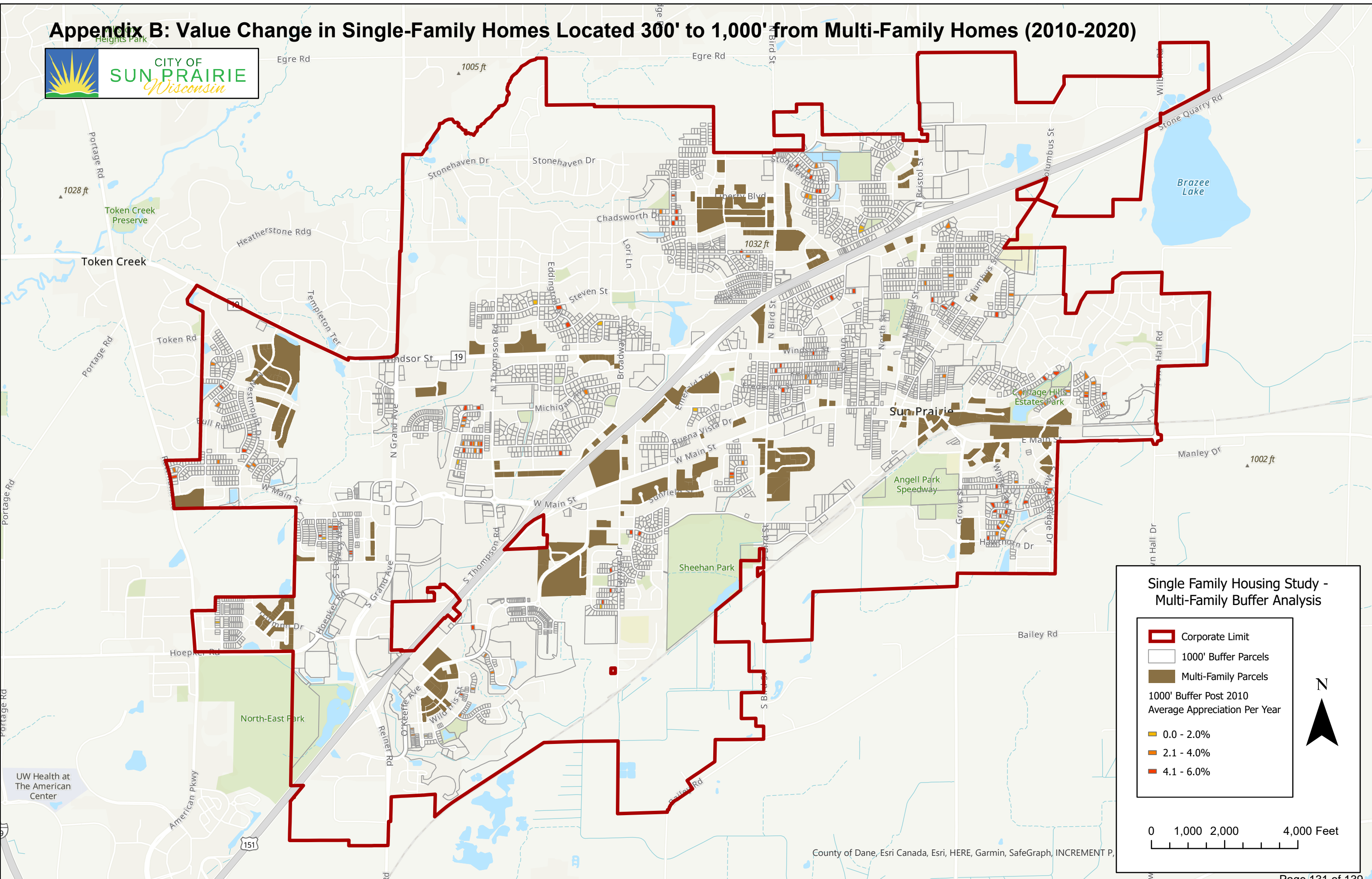
N

0 1,000 2,000 4,000

Feet

County of Dane, Esri Canada, Esri, HERE, Garmin, SafeGraph, INCREMENT P, METI/NASA, USGS, EPA, NPS, U

Appendix B: Value Change in Single-Family Homes Located 300' to 1,000' from Multi-Family Homes (2010-2020)

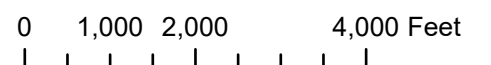


Single Family Housing Study - Multi-Family Buffer Analysis

- Corporate Limit
- 1000' Buffer Parcels
- Multi-Family Parcels

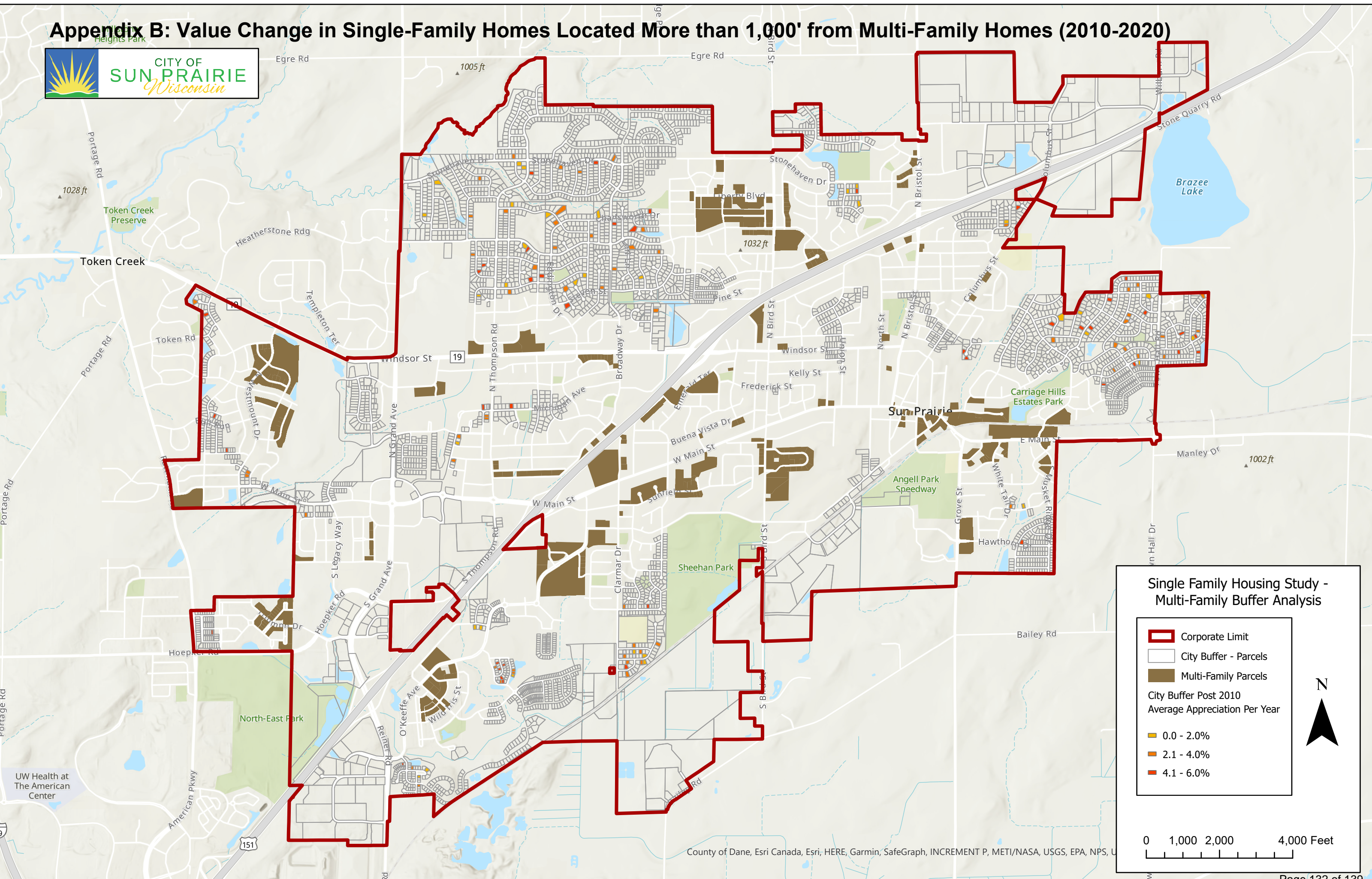
1000' Buffer Post 2010 Average Appreciation Per Year

- 0.0 - 2.0%
- 2.1 - 4.0%
- 4.1 - 6.0%



County of Dane, Esri Canada, Esri, HERE, Garmin, SafeGraph, INCREMENT P,

Appendix B: Value Change in Single-Family Homes Located More than 1,000' from Multi-Family Homes (2010-2020)

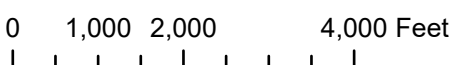


Single Family Housing Study - Multi-Family Buffer Analysis

- Corporate Limit
- City Buffer - Parcels
- Multi-Family Parcels

City Buffer Post 2010
Average Appreciation Per Year

- 0.0 - 2.0%
- 2.1 - 4.0%
- 4.1 - 6.0%



County of Dane, Esri Canada, Esri, HERE, Garmin, SafeGraph, INCREMENT P, METI/NASA, USGS, EPA, NPS, U.S. Geological Survey

PROJECT Village of Mount Horeb – General Review DATE 11/26/2025

PROJECT NO. 00050581.001

SUBJECT Langhus GDP Review

PREPARED BY Robert Wright

NAME	COMPANY
Nic Owen – Village Administrator	Village of Mount Horeb
Ben Rohr – Village Planner	Vandewalle & Associates

NOTES

We have reviewed the information supplied by the Developer (JG Development) related to the CSM, Traffic Impact Analysis, and GDP for the 4-story mixed use Building at 210.214 and 220 E. Main Street. The developer’s proposal has been revised with removal of approximately 22 units for a total unit count of 44 (66 in original proposal). As we had previously discussed stormwater and utility connections and they have remained unaffected by the revised proposal, this review will focus on potential Traffic Impact

TRAFFIC IMPACT

A Traffic Impact Analysis (TIA) was submitted by the Developer 8/14/2025. The traffic counts were taken during summer (non-school day traffic), however they fall closely in line with past traffic counts completed by WISDOT and the MHPD. During public hearings related to the original project proposals, there were comments related to school vs. non-school session traffic analysis (Summer vs. Fall). Because the analysis was completed in the summer (non-school) along with the MHPD study, we will focus on the study as presented. Also, the original study assumed a full residential buildout of 66 units, while the revised proposal will be for 44 units. We should not assume that the entire 1/3 of the traffic to be reduced from what was presented originally, however this may be a close approximation for this review.

The site will be accessing off the alley parallel to Main Street and off N. 3rd Street. The Zoning Adjacent to 3rd is Zoned CB, and allows for direct access to this roadway.

Trip generation for the development follows the ITE and were limited to 220 total trips per day (110 in and 110 out) 30 of these trips were Peak hour AM, and 25 during Peak hour PM. This will be recued with the current proposal.

Current Level of Service (LOS) for the area for the 2nd/Main and 3rd/Main intersection were functioning at or above a LOS of D, the development does cause an additional decrease of LOS (LOS from D to E) in one scenario at 3rd/Main southbound from 3rd to Main, with an increase in delay time of about 7-seconds. Delay and queuing time are still within reasonable amounts. This should decrease in relation to the reduced unit counts and potentially provide a higher LOS based on reduced trip generation.

Review of the parking spots on the east side of 2nd and the west side of 3rd should be undertaken to adhere to Wisconsin State Statute 346: Rules of the Road for parking sizes; clearance to alley/drives; and clearance to stop/signs and crosswalks. This may result in removal of parking spots to follow Wis. Stats. 346.

As the pedestrian crossing and issue is important to the Village as a whole, some of the existing pedestrian issues are both standalone and potentially further impacted by the development. We continue to suggest additional pedestrian crossing improvements should be made to the Main Street crossings due to high pedestrian usage. We have been in discussions with MHPD and Public Services related to signage and marking in response to current residents' concerns.












CERTIFIED SURVEY MAP NO. _____

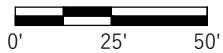
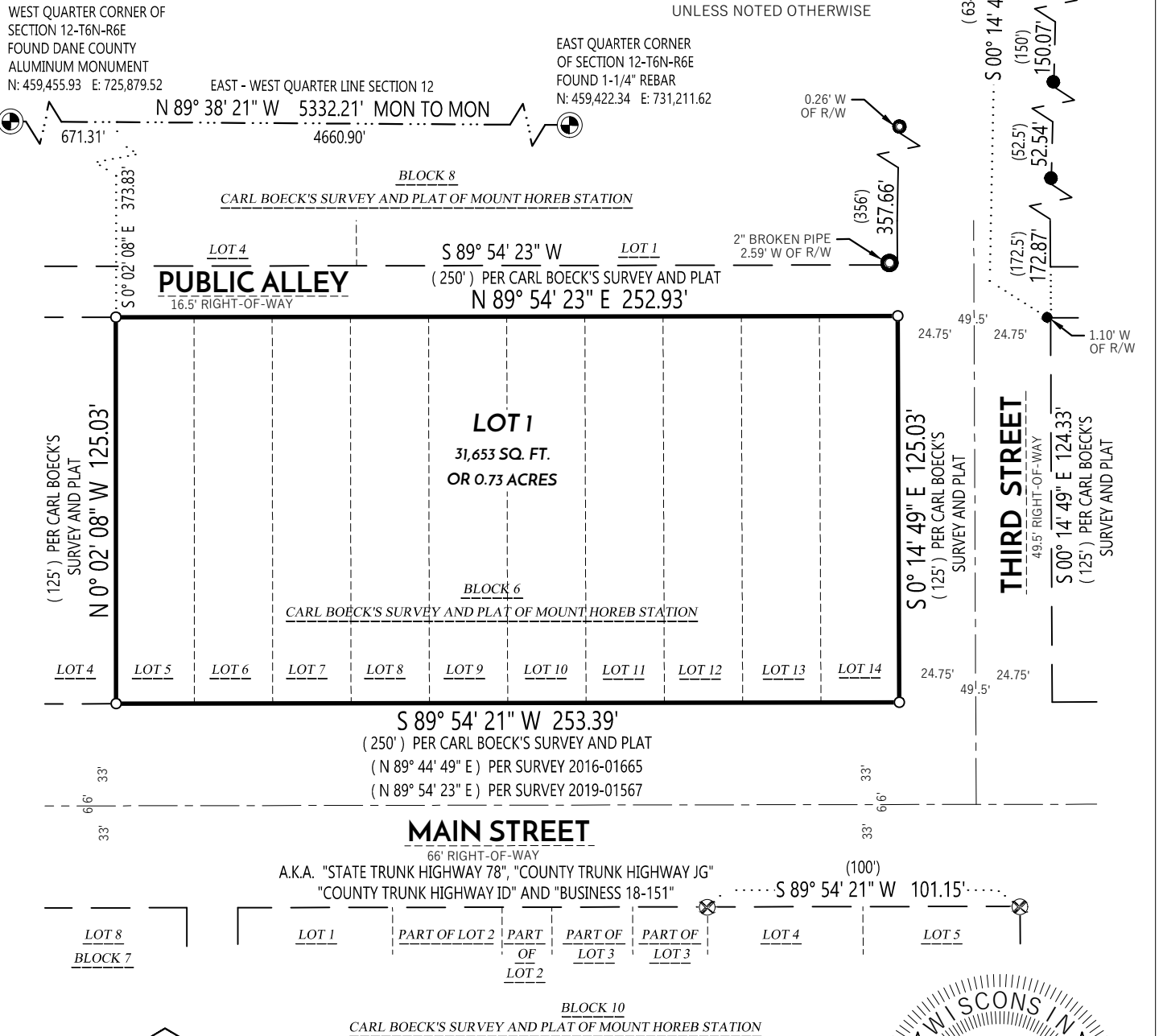
A CONSOLIDATION OF LOTS 5-14, BLOCK 6, CARL BOECK'S SURVEY AND PLAT OF MOUNT HOREB STATION, RECORDED IN VOLUME A, PAGE 41, AS DOCUMENT 157120, BEING LOCATED IN THE NORTHWEST QUARTER OF THE SOUTHWEST QUARTER OF SECTION 12, TOWNSHIP 6 NORTH, RANGE 6 EAST, VILLAGE OF MOUNT HOREB, DANE COUNTY, WISCONSIN.

NOTES:

1. FIELD WORK PERFORMED BY WYSER ENGINEERING, LLC. ON MAY 27TH AND 28TH, 2025, AND JUNE 13TH, 2025.
2. NORTH REFERENCE FOR THIS CERTIFIED SURVEY AND MAP ARE BASED ON THE WISCONSIN COORDINATE REFERENCE SYSTEM, WISCRS DANE, NAD 83 (2011), GRID NORTH. THE EAST-WEST QUARTER LINE OF SECTION 12-T6N-R6E, WAS MEASURED TO BEAR S 89° 54' 21" W.
3. THIS PARCEL IS SUBJECT TO ALL EASEMENTS AND AGREEMENTS, BOTH RECORDED AND UNRECORDED.
4. SEE SHEET 2 OF 4 FOR FURTHER DETAILS ON EXISTING CONDITIONS AND EASEMENTS.
5. THE MAIN STREET RIGHT-OF-WAY IS BASED ON FOUND IRONS/MARKINGS AS SHOWN ON PREVIOUS SURVEY PERFORMED BY WYSER ENGINEERING, DANE CO. RECORDS SURVEY 2016-01665, DATED 10/12/2016.

LEGEND

-  SECTION CORNER FOUND
-  3/4" REBAR FOUND, UNLESS OTHERWISE NOTED
-  1" IRON PIPE FOUND, UNLESS OTHERWISE NOTED
-  CUT CROSS FOUND
-  3/4" X 18" REBAR SET, WT. 1.50 LBS/FT MIN.
-  CSM BOUNDARY
-  RIGHT-OF-WAY LINE
-  CENTERLINE
-  SECTION/QUARTER LINE
-  PLATTED LINE
-  () RECORDED INFORMATION PER CARL BOECK'S SURVEY AND PLAT, UNLESS NOTED OTHERWISE

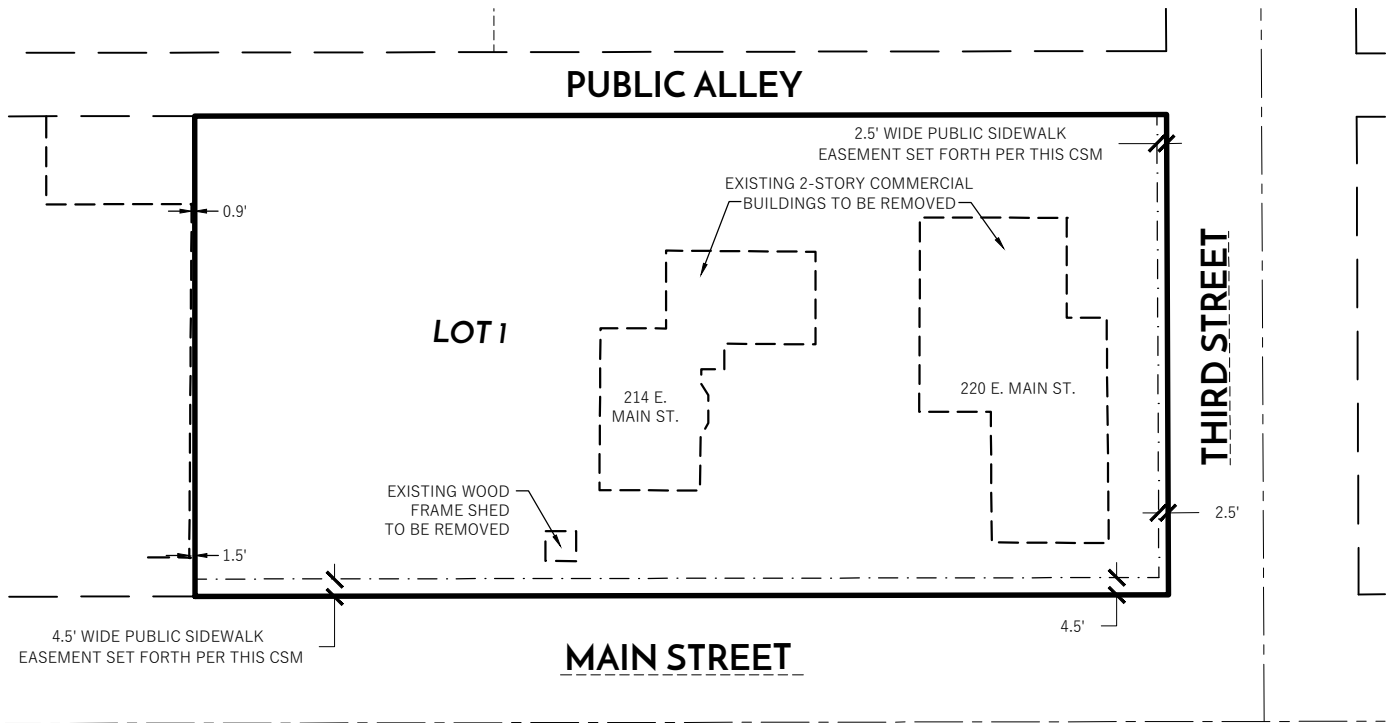


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<p>WYSER ENGINEERING</p> <p>PREPARED BY: WYSER ENGINEERING 300 EAST FRONT STREET MOUNT HOREB, WI 53572 www.wyserengineering.com</p>	<p>PREPARED FOR: JG DEVELOPMENT 455 S. JUNCTION RD. MADISON, WI 53719</p>	<p>SURVEYED BY: MAL DRAWN BY: SCH CHECKED BY: ZMR APPROVED BY: ZMR</p>	<p>VOL. _____ PAGE _____</p> <p>DOC. NO. _____</p> <p>C.S.M. NO. _____</p> <p>PROJECT NO: 251430 SHEET NO: 1 of 4</p>
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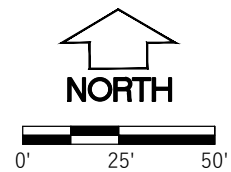
CERTIFIED SURVEY MAP NO. _____

A CONSOLIDATION OF LOTS 5-14, BLOCK 6, CARL BOECK'S SURVEY AND PLAT OF MOUNT HOREB STATION, RECORDED IN VOLUME A, PAGE 41, AS DOCUMENT 157120, BEING LOCATED IN THE NORTHWEST QUARTER OF THE SOUTHWEST QUARTER OF SECTION 12, TOWNSHIP 6 NORTH, RANGE 6 EAST, VILLAGE OF MOUNT HOREB, DANE COUNTY, WISCONSIN.



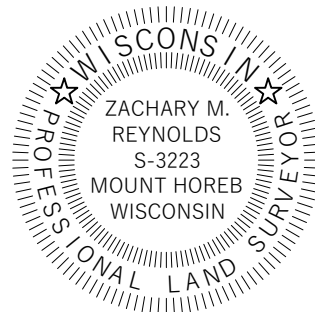
LEGEND

- CSM BOUNDARY
- RIGHT-OF-WAY LINE
- CENTERLINE
- PLATTED LINE
- BUILDING FOOTPRINT
- EASEMENT CREATED PER THIS CSM



NOTES:

1. THIS PARCEL IS SUBJECT TO ALL EASEMENTS AND AGREEMENTS, BOTH RECORDED AND UNRECORDED.



File: W:\2025\251430_JG Development - Olsons Development, Mount Horeb\DWG\251430_CSM.dwg Layout: CSM 2 OF 4 User: zreynolds Plotted: Sep 22, 2025 -- 4:21pm



PREPARED BY:
WYSER ENGINEERING
300 EAST FRONT STREET
MOUNT HOREB, WI 53572
www.wyserengineering.com

PREPARED FOR:
JG DEVELOPMENT
455 S. JUNCTION RD.
MADISON, WI 53719

SURVEYED BY: MAL
DRAWN BY: SCH
CHECKED BY: ZMR
APPROVED BY: ZMR

PROJECT NO: 251430
SHEET NO: 2 of 4

VOL. _____ PAGE _____
DOC. NO. _____
C.S.M. NO. _____

CERTIFIED SURVEY MAP NO. _____

A CONSOLIDATION OF LOTS 5-14, BLOCK 6, CARL BOECK'S SURVEY AND PLAT OF MOUNT HOREB STATION, RECORDED IN VOLUME A, PAGE 41, AS DOCUMENT 157120, BEING LOCATED IN THE NORTHWEST QUARTER OF THE SOUTHWEST QUARTER OF SECTION 12, TOWNSHIP 6 NORTH, RANGE 6 EAST, VILLAGE OF MOUNT HOREB, DANE COUNTY, WISCONSIN.

LEGAL DESCRIPTION

A CONSOLIDATION OF LOTS 5-14, BLOCK 6, CARL BOECK'S SURVEY AND PLAT OF MOUNT HOREB STATION, RECORDED IN VOLUME A OF PLATS, PAGE 41, AS DOCUMENT NO. 157120, BEING A PART OF THE NORTHWEST QUARTER OF THE SOUTHWEST QUARTER OF SECTION 12, TOWN 6 NORTH, RANGE 6 EAST, ALL IN THE VILLAGE OF MOUNT HOREB, DANE COUNTY, WISCONSIN, DESCRIBED MORE PARTICULARLY AS FOLLOWS:

COMMENCING AT THE WEST QUARTER CORNER OF AFORESAID SECTION 12, THENCE, ALONG THE NORTH LINE OF THE SOUTHWEST QUARTER OF SAID SECTION 12, SOUTH 89 DEGREES 38 MINUTES 21 SECONDS EAST, 671.31 FEET; THENCE, SOUTH 00 DEGREES 02 MINUTES 08 SECONDS EAST, 373.83 FEET TO THE NORTHWEST CORNER OF AFORESAID LOT 5 AND THE POINT OF BEGINNING;

THENCE, ALONG THE NORTH LINE OF AFORESAID BLOCK 6, NORTH 89 DEGREES 54 MINUTES 23 SECONDS EAST, 252.93 FEET TO THE NORTHEASTERN CORNER OF AFORESAID LOT 14, ALSO BEING A POINT ON THE WESTERLY RIGHT-OF-WAY OF THIRD STREET; THENCE, ALONG THE SAID WESTERLY RIGHT-OF-WAY AND THE EASTERLY LINE OF SAID LOT 14, SOUTH 00 DEGREES 14 MINUTES 49 SECONDS EAST, 125.03 FEET TO THE SOUTHEAST CORNER OF SAID LOT 14, ALSO BEING A POINT ON THE NORTH RIGHT-OF-WAY OF MAIN STREET; THENCE, ALONG THE SAID NORTHERLY RIGHT-OF-WAY LINE AND SOUTHERLY LINE OF AFORESAID BLOCK 6, SOUTH 89 DEGREES 54 MINUTES 21 SECONDS WEST, 253.39 FEET TO THE SOUTHWESTERN CORNER OF AFORESAID LOT 5; THENCE, ALONG THE WESTERLY LINE OF SAID LOT 5, NORTH 00 DEGREES 02 MINUTES 08 SECONDS WEST, 125.03 FEET BACK TO THE POINT OF BEGINNING.

SAID PARCEL CONTAINS 31,653 SQUARE FEET OR 0.73 ACRES.

SURVEYOR'S CERTIFICATE

I, ZACHARY M. REYNOLDS, WISCONSIN PROFESSIONAL LAND SURVEYOR S-3223, DO HEREBY CERTIFY THAT BY DIRECTION OF JEFF GRUNDAHL, JG DEVELOPMENT, I HAVE SURVEYED, DIVIDED, AND MAPPED THE LANDS DESCRIBED HEREON AND THAT THE MAP IS A CORRECT REPRESENTATION IN ACCORDANCE WITH THE INFORMATION PROVIDED. I FURTHER CERTIFY THAT THIS CERTIFIED SURVEY MAP IS IN FULL COMPLIANCE WITH CHAPTER 236.34 OF THE WISCONSIN STATUTES AND THE SUBDIVISION REGULATIONS OF THE VILLAGE OF MOUNT HOREB AND DANE COUNTY, WISCONSIN.

ZACHARY M. REYNOLDS, S-3223
WISCONSIN PROFESSIONAL LAND SURVEYOR

DATE



File: W:\2025\251430_JG Development - Olsons Development, Mount Horeb\DWG\251430_CSM.dwg Layout: CSM 3 OF 4 User: zreynolds Plot: Sep 22, 2025 4:20pm



PREPARED BY:
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MADISON, WI 53719

SURVEYED BY: MAL
DRAWN BY: SCH
CHECKED BY: ZMR
APPROVED BY: ZMR

PROJECT NO: 251430
SHEET NO: 3 of 4

VOL. _____ PAGE _____
DOC. NO. _____
C.S.M. NO. _____

CERTIFIED SURVEY MAP NO. _____

A CONSOLIDATION OF LOTS 5-14, BLOCK 6, CARL BOECK'S SURVEY AND PLAT OF MOUNT HOREB STATION, RECORDED IN VOLUME A, PAGE 41, AS DOCUMENT 157120, BEING LOCATED IN THE NORTHWEST QUARTER OF THE SOUTHWEST QUARTER OF SECTION 12, TOWNSHIP 6 NORTH, RANGE 6 EAST, VILLAGE OF MOUNT HOREB, DANE COUNTY, WISCONSIN.

OWNER'S CERTIFICATE

214 E MAIN LLC, AS OWNER, WE HEREBY CERTIFY THAT WE CAUSED THE LANDS DESCRIBED HEREON TO BE SURVEYED, DIVIDED MAPPED AND DEDICATED AS SHOWN. WE ALSO CERTIFY THAT THIS CERTIFIED SURVEY MAP IS REQUIRED BY S. 236.34 OF THE WISCONSIN STATE STATUTES TO BE SUBMITTED TO THE VILLAGE OF MOUNT HOREB FOR APPROVAL.

BY: _____
MANAGING MEMBER
214 E MAIN LLC

STATE OF WISCONSIN) SS
DANE COUNTY) SS

PERSONALLY CAME BEFORE ME THIS _____ DAY OF _____, 2025, THE ABOVE

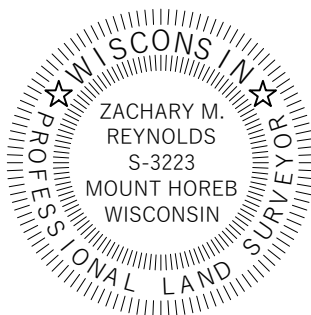
NAMED, MANAGING MEMBER OF 214 E MAIN LLC, _____, TO ME KNOWN TO BE THE PERSON WHO EXECUTED THE FOREGOING INSTRUMENT, AND ACKNOWLEDGED THE SAME.

NOTARY PUBLIC, STATE OF WISCONSIN MY COMMISSION EXPIRES _____

VILLAGE OF MOUNT HOREB APPROVAL

THE VILLAGE OF MOUNT HOREB HAS APPROVED THIS CERTIFIED SURVEY MAP.

ALYSSA GAFFNEY DATE _____
VILLAGE CLERK, VILLAGE OF MOUNT HOREB



PREPARED BY:
WYSER ENGINEERING
300 EAST FRONT STREET
MOUNT HOREB, WI 53572
www.wyserengineering.com

PREPARED FOR:
JG DEVELOPMENT
455 S. JUNCTION RD.
MADISON, WI 53719

SURVEYED BY: MAL
DRAWN BY: SCH
CHECKED BY: ZMR
APPROVED BY: ZMR

PROJECT NO: 251430
SHEET NO: 4 of 4

OFFICE OF THE REGISTER OF DEEDS

COUNTY, WISCONSIN
RECEIVED FOR RECORD _____
20 ____ AT _____ O'CLOCK ____ M AS
DOCUMENT # _____
IN VOL. _____ OF CERTIFIED SURVEY
MAPS ON PAGE(S) _____

KRISTI CHLEBOWSKI, REGISTER OF DEEDS

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AGENDA ITEM REPORT

MEETING DATE

January 7, 2026

PREPARED BY

Nicholas Owen, Administrator

AGENDA ITEM # 11.a

Consideration of TIF Incentive Request (Jeff Grundahl Main Street Mixed Use Project). The Village Board may convene in closed session as authorized by Wisconsin Statute 19.85(1)(e) for the purpose of deliberating or negotiating the investing of public funds or conducting other specified public business, whenever competitive or bargaining reasons require a closed session.

BACKGROUND

The CDA will have a closed session discussion on confidential financial information regarding the request for TIF assistance.

RECOMMENDATION

ATTACHMENTS

1. MT Horeb - Incentive Request